TOWN OF BROOKLYN PLANNING AND ZONING COMMISSION Regular Meeting Agenda Wednesday, August 5, 2020 6:30 p.m.

To join this meeting via the web or phone, follow the below instructions:		
Web	Phone	
Go to www.webex.com	Dial 1-408-418-9388	
On the top right, click Join	Enter meeting number: 129 396 8633	
Enter meeting information: 129 396 8633	You can bypass attendee number by	
Enter meeting password: 98miCH591YT	pressing #	
Click join meeting		

- I. Call to Order
- II. Roll Call
- **III.** Seating of Alternates
- IV. Adoption of Minutes: Special Meeting July 8, 2020
- V. Public Commentary
- VI. Unfinished Business:
 - a. Reading of Legal Notice
 - b. New Public Hearings:
 - 1. **SPG 20-001** Gravel Special Permit, Paul R. Lehto, 71.34 acres on the east side of Allen Hill Road (Map 32, Lot 148) in the RA Zone; Excavation of approximately 90,000 cubic yards of sand and gravel on 6.7 acres.
 - c. Continued Public Hearings: None.
 - d. Other Unfinished Business:
 - 1. Discussion with Margaret Washburn, ZEO
 - SPG 20-001 Gravel Special Permit, Paul R. Lehto, 71.34 acres on the east side of Allen Hill Road (Map 32, Lot 148) in the RA Zone; Excavation of approximately 90,000 cubic yards of sand and gravel on 6.7 acres.

VII. New Business:

a. Applications:

- 1. **SD 20-002** 3-lot Subdivision, Applicant: David and Nancy Bell, 25.65 acres on the east side of Church St. (Map 35, Lot 4) in the RA Zone; Proposed creation of 3 residential buildings lots on a common driveway.
- 2. **SD 20-003** 3-lot Subdivision, Applicant: David and Nancy Bell, 6 acres on the east side of Prince Hill Road (131 Prince Hill Road, Map 34, Lot 52) in the RA Zone; Proposed creation of 3 residential buildings lots, two sharing a common driveway.
- 3. **ZC 20-002** Zone Boundary Change from R-30 to RA, Applicant: Keith Crossman, 340 Christian Hill Road, proposed adjustment to 6.75 acres on east side of Christian Hill Road.
- SP 20-002 Special Permit for additional vehicle storage, Applicant: Vachon Brooklyn, LLC, 512 Providence Road, Proposed construction of two 16' wide access drives to proposed new vehicle storage lots.

b. Other New Business:

VIII. Reports of Officers and Committees:

- a. Staff Reportsb. Budget Update
- c. Correspondence.
- d. Chairman's Report.

IX. **Public Commentary**

X. Adjourn

Michelle Sigfridson, Chairman

TOWN OF BROOKLYN PLANNING AND ZONING COMMISSION Special Meeting Wednesday, July 8, 2020 Clifford B. Green Meeting Center 69 South Main Street 6:30 p.m.

To join this meeting via the web or phone, follow the below instructions:			
Web	Phone		
Go to www.webex.com	Dial 1-408-418-9388		
On the top right, click Join	Enter meeting number: 129 532 5897		
Enter meeting information: 129 532 5897	You can bypass attendee number by pressing #		
Enter meeting password: MArw5729TY87			
Click join meeting			

MINUTES

- I. Call to Order Michelle Sigfridson, Chair, called the meeting to order at 6:38 p.m.
- **II. Roll Call** Michelle Sigfridson, Carlene Kelleher, Earl Starks, Alan Fitzgerald, Austin Tanner, Charles Sczuroski was absent with notice.

Staff Present: Jana Roberson, Director of Community Development; Richard Ives, First Selectman and ex officio Member of the PZC (left at 8:45 p.m.).

Also Present: Paul Archer, Archer Surveying; David Held, Provost & Rovero; David Wilterdink; Jeff Weaver.

III. Seating of Alternates – None.

IV. Adoption of Minutes: Regular Meeting June 3, 2020

Motion was made by A. Tanner to approve the Minutes of the Regular Meeting of June 3, 2020. Second by E. Starks. No discussion.

Roll Call Vote: C. Kelleher – yes; A. Tanner – yes; E. Starks – yes; A. Fitzgerald – yes; M. Sigfridson – yes. Motion carried unanimously (5-0-0).

V. **Public Commentary** – None.

VI. Unfinished Business:

- **a. Reading of Legal Notice:** Jana Roberson read aloud the Legal Notice for ZC 20-001 and ZRC 20-001 rev. It had been published in the *Villager* Newspaper and it was posted on the website.
- b. New Public Hearings: None.
 - 1. **ZC 20-001** Zone Boundary Change to R-30/RA boundary on south side of Day St., Applicant: Jeff Weaver, proposed adjustment to match proposed lot lines in subdivision.

Paul Archer, Archer Surveying, represented the Applicant (also present) and gave an overview. Mr. Archer explained that they would like to revise some of the zone boundaries to include all of the lots. He explained that one of the lots in the subdivision (on the east side) is split in half by the RA/R-30 Zone. The southerly line is approximately 25 feet short of the R-30/RA Zone. They would like to modify the R-30 Zone so that it would encompass all of the lots that are in the R-30 Zone. This would entail moving the R-30 line 50 feet to the east and roughly 20 feet to the south.

Plans were displayed as discussed.

COMMENTS FROM STAFF:

Jana Roberson explained that the boundary change would be approximately one acre and that the zone boundary pre-existed the laying out of lot lines, so there was really no lot line to follow at the time that the zones were laid out. This is a very minor adjustment to the existing zoning.

COMMENTS FROM COMMISSION MEMBERS:

A.Tanner asked about the proposed access on the east side. He asked if part of it is outside of the lot.

Mr. Archer stated that the front half access strip is in the RA Zone and that the back half is in the R-30 Zone. He said that the intent is for proposed Lot 2 to be totally in the R-30 Zone.

Mr. Tanner asked if part is being changed that is not proposed lots.

Ms. Roberson displayed and explained a map depicting the area of the zone boundary change. She explained that the way that proposed Lot 2 is laid out, the zone boundary went right through it and it did not meet the lot size requirements for the RA Zone, so the zone boundary would have to shift. This would be required in order to move forward with the subdivision with Lot 2 in this current configuration.

Mr. Tanner asked if making the line straight to the west has anything to do with the lots (above the remaining land).

Mr. Archer explained that the intent is to make the zone line straight.

Motion was made by A. Tanner to close the public hearing for **ZC 20-001** – Zone Boundary Change to R-30/RA boundary on south side of Day St., Applicant: Jeff Weaver, proposed adjustment to match proposed lot lines in subdivision. Second by C. Kelleher. No discussion.

Roll Call Vote: A. Tanner – yes; E. Starks – yes; A. Fitzgerald – yes; C. Kelleher – yes; M. Sigfridson – yes. Motion carried unanimously (5-0-0).

ZRC 20-001 rev – A proposal to make amendments to the Zoning Regulations concerning accessory buildings, excavation operations, and other various corrections including Sec. 3.A.5.2.1, 3.B.5.2.1, 3.C.5.2.1, 4.A.4.2.1, 4.B.4.2.1, 4.C.4.2.1, 3.C.2.4.5, 4.D.2.1.5, 6.K.2.2, 6.O.4.1, 6.P.3.3, 7.B.5.4.

Jana Roberson gave an overview:

- Some are technical corrections.
- Currently, accessory buildings are not allowed to be located in a front yard in multiple zones. She explained that there is a proposal to allow accessory buildings in the front yard by special permit.
- There is a change proposed to remove the requirement for a \$500 financial guarantee for donations bins put out by a tax-exempt organization. Currently, there is no such exemption.
- There is a proposed change to the standards for excavation operations in the gravel mining section of the Regulations (6.O.4.1). She read aloud the following proposed language which, she said, is returning old language to the Regulations. "The minimum elevation of the excavation shall be no less than five feet above seasonal high groundwater and no less than six feet above ledge. Approval of the creation of a pond or water body requires a separate vote of the Commission."
- There is a proposed change regarding an oversight in Section 6.P.3.3 Earth Materials Processing, as follows: "All processed materials must be clean as defined by CT DEEP and free from any solid waste."

There was discussion regarding accessory buildings.

Ms. Roberson explained that she had researched other towns to see how they handle it and, of the responses she received: one-third allow accessory buildings in the front yard; one-third allow only by special permit; and one-third allow it, but under specific criteria unique to that town depending on the circumstances (and she gave some examples). She explained that the front yard is the space between the primary structure and the road.

M. Sigfridson stated that this topic had been discussed at a previous meeting and that her recollection was that there was a desire among most of the Commission Members to be a little less strict than not allowing any accessory buildings/structures in the front yard, but there was some trepidation as to what it would look like and how to go about it and, therefore, came up with the idea to allow it by special permit. There were no comments from the Commission Members. Ms. Sigfridson explained that she likes the idea of special permit, except for the cost.

Rick Ives stated that he is in favor, but would prefer that it be done by setbacks rather than by special permit. He feels this would be less complicated.

Paul Archer stated agreement with Mr. Ives. Mr. Archer feels that there hasn't been a problem with accessory buildings over the last 20 years and he suggested going back to the old language which, she said, allows accessory buildings outside the setbacks. He said to allow the setbacks do what they are intended to do.

Mr. Ives stated agreement with Mr. Archer.

David Wilterdink stated that he would like to put an accessory building on his corner lot which has a large front yard and no back yard.

Mr. Archer explained that there is an existing accessory building (garage) in the front yard, which was allowed back then. He said that that Mr. Wilterdink had been given guidance that he would need to get a variance, which Mr. Archer feels is correct due to the current Regulations. Mr. Wilterdink explained that, as part of the variance procedure, he had Mr. Archer do a survey of his property.

C. Kelleher asked if someone were to want to put an accessory structure anywhere, would they have to speak with the ZEO?

Mr. Archer stated that they would need a zoning permit to put the building in. Ms. Kelleher stated that, rather than special permit, she would be comfortable as long as they go to the ZEO to be sure that they are within the required setbacks and the ZEO could alert the PZC if there was something that could not be allowed.

Ms. Sigfridson stated that she agrees that this hasn't been a problem in Town. She feels that the issue is with aesthetic concerns (e.g. pools, chicken coops, shipping containers). She suggested the possibility of not allowing shipping containers. Discussion ensued regarding shipping containers and whether the definitions for accessory buildings and accessory structures include shipping containers. Ms. Roberson read aloud the definitions from the Zoning Regulations. Mr. Fitzgerald stated that the definition for accessory building takes care of his concerns. Discussion continued and site plan approval was considered. Mr. Archer suggested that the issue is not with accessory buildings, but rather with shipping containers. Ms. Roberson's opinion is that a shipping container is a structure. Ms. Sigfridson's opinion is that a shipping container is a building and she stated agreement with Mr. Archer that the main concern seems to be with shipping containers in the front yard. Mr. Ives stated that if the concern is with shipping containers, then just don't allow shipping containers anywhere (or perhaps just in the back yard). Ms. Sigfridson suggested that shipping containers, specifically, could not be considered a building for PZC purposes. She feels that it doesn't make sense to disallow them everywhere. Mr. Fitzgerald stated that he has no problem with a garage in the front yard, but he doesn't want to see shipping containers. Discussion continued. Ms. Roberson noted that there is a definition of container (which doesn't reference structure or building) in the Zoning Regulations and they are currently, specifically, only permitted as an accessory in one zone (Restricted Business). They are not allowed in the residential zones because it is not a listed permitted use. Ms. Sigfridson noted that "portable" is the key word and she suggested deleting the prohibition on accessory buildings in the front yard, without deciding whether a container is a building or not, and to say, specifically, that under those circumstances would a container be permitted in the front yard. A. Tanner suggested a time limit such as 30 days.

Ms. Sigfridson reviewed current language. Mr. Archer suggested that the Commission look at Section 17.2 of the old regulations (Accessory Buildings). Ms. Sigfridson suggested, z Zoning Commission from the current Regulations, the table of setback standards for the front yard on page 39 of R-10 (make it match the setbacks for the principle building).

There was discussion regarding whether to prohibit containers in the front yard. Ms. Roberson feels that it is already covered. Mr. Ives suggested that the change regarding accessory buildings be done tonight and to form a strong opinion regarding the containers to be further discussed at the next meeting. There was agreement expressed by M. Sigfridson and A. Fitzgerald.

Ms. Sigfridson asked if there were any other items, other than technical corrections, that require discussion. Mr. Ives stated he hopes that everyone supports the change regarding Donation Bins. Ms. Kelleher stated that the Commission had been in agreement at a prior meeting. There were no further comments.

Motion was made by A. Fitzgerald to close the public hearing for **ZRC 20-001 rev** – A proposal to make amendments to the Zoning Regulations concerning accessory buildings, excavation operations, and other various corrections including Sec. 3.A.5.2.1, 3.B.5.2.1, 3.C.5.2.1, 4.A.4.2.1, 4.B.4.2.1, 4.C.4.2.1, 3.C.2.4.5, 4.D.2.1.5, 6.K.2.2, 6.O.4.1, 6.P.3.3, 7.B.5.4. Second by C. Kelleher. No discussion. Roll Call Vote: E. Starks – yes; A. Fitzgerald – yes; C. Kelleher – yes; A. Tanner – yes; M. Sigfridson – yes. Motion carried unanimously (5-0-0).

c. Continued Public Hearings: None.

d. Other Unfinished Business:

1. **ZC 20-001** – Zone Boundary Change to R-30/RA boundary on south side of Day St., Applicant: Jeff Weaver, proposed adjustment to match proposed lot lines in subdivision.

Motion was made by C. Kelleher to approve **ZC 20-001** – Zone Boundary Change to R-30/RA boundary on south side of Day St., Applicant: Jeff Weaver, proposed adjustment to match proposed lot lines in subdivision, with the finding that it is suitable for the location, will aid in the protection of public health, safety, welfare and property values and is consistent with the Plan of Conservation and Development and the intent of the Zoning Regulations. The zone boundary change shall become effective 15 days from the date of publication on the website. Second by E. Starks. No discussion.

Roll Call Vote: A. Fitzgerald – yes; C. Kelleher – yes; A. Tanner – yes; E. Starks – yes; M. Sigfridson – yes. Motion carried unanimously (5-0-0).

2. **SD 20-001** – 6-Lot Subdivision, Applicant: Jeff Weaver, 8 acres on south side of Day St., (Map 43, Lot 6) in the RA and R-30 Zones; Proposed creation of six residential lots.

Plans/photos/documents were displayed as discussed.

Paul Archer, Archer Surveying, represented the Applicant (also present) and gave an overview:

- Mr. Archer noted that they have been before the PZC with preliminary discussion and, therefore, the Commission has an idea of what the Applicant is doing. They are proposing a six-lot subdivision to the west side of the ten-lot subdivision that they did at the beginning of the year.
- Now that the zone boundary has been approved, they are proposing four lots in the R-30 Zone and two lots in the RA Zone.
- Two of the lots will involve a 50-foot access strip which will give access to two rear lots (one in the R-30 Zone and one in the RA Zone).
- IWWC approval has been obtained.
- NDDH approval has been obtained.
- They are proposing a fee-in-lieu of open space. Platt Associates did the appraisal of roughly nine acres of land being subdivided (\$60,000). They are proposing a fee-in-lieu of \$6,000 to be divided among the six lots.
- Mr. Archer stated that some of Syl Pauley's comments (dated April 23, 2020), from his review, had been addressed and some have not been addressed:

SHEET 3 OF 6 – SITE DEVELOPMENT PLAN

- Wells for Lots 2 thru 4 are very close to Day Street... Mr. Archer stated that the Health Department should determine the well locations.
- 2. The vast majority of Lot 1 is in the regulated wetland upland review area... Mr. Archer stated that the IWWC didn't have a problem with it and they approved it.

SHEET 4 OF 6 - DETAIL SHEET NO. 1

- Proposed grading along the east side of the proposed driveway... Mr. Archer stated that David Held of Provost and Rovero designed it and he felt that the grading on the side of the driveway was adequate enough to do it.
- Development of the site will be the responsibility of the lot owner... Mr. Archer explained that if Mr. Weaver decides to sell the lots prior to developing them, then the lot owner that has it is required to do it.

SHEET 5 OF 6 - DETAIL SHEET NO. 2

- 1. A utility trench detail should be included for any underground utility installation. A water service trench detail should also be included. Both details should include detectable warning tape.
- 2. A typical detail of the foundation underdrain size, material, pipe class strength, rodent protected outlet and riprap splash pad should be included on this sheet.

Mr. Archer stated that if the Commission feels that these details need to be added, they would have no problem adding them.

Mr. Archer stated that he had received comments from Ms. Roberson on July 7, 2020:

- Neighboring land owners had not been notified of this proposal as per Section 2.1 of the Subdivision Regulations.
 Mr. Archer explained that the neighbors were to notified 10-15 days prior to the application being submitted. He said that sometimes this is followed and sometimes it is not. He said that they had not notified the neighbors 10-15 days prior to the application being submitted.
- Proper road width has not been dedicated to the Town.
 Mr. Archer explained that the road width, if you're coming off the centerline of the road, the Town owns to the face of the wall. Most of the time it was changing by an average of one-to-two feet. He did not see the need to deed over to the Town one-to-two feet of land which is basically going to take the property line from the front of the stone wall to the back of the stone wall. It wasn't the whole length, it was just in spots. Mr. Weaver has no problem dedicating it to the Town if the PZC requires it.
- No Site Analysis Plan has been submitted.
 - Mr. Archer stated that it is good soil and he explained that when they did the 20acre subdivision next door, the Conservation Commission did not want open space at that time. He said that the Conservation Commission did not understand that Mr. Weaver was possibly going to subdivide the 60+ acres that he has now or they may have had a different opinion. Mr. Archer stated that they are proposing fee-inlieu of open space.
- Archeological sensitivity is unknown.
 - Mr. Archer explained that for the 20-acres, Mr. Weaver had archeological study done and it showed that in the lower regions, where they dug 60-64 holes, they found nothing and that all of the surrounding areas would be either not affected or a lower advantage because findings would be in the lower flats or in the side of the hills.
- Property boundaries do not follow existing stone walls. Mr. Archer explained that this was Mr. Weaver's choice.
- Storm water runoff exceeds the capacity of the wetlands creating an increased risk of flooding on Day Street.

Mr. Archer stated that he does not see that and he explained that the way the subdivision is designed is that all of the water from all of the houses and driveways is staying 100 percent on his property and he is not pushing water onto Day Street.

Mr. Archer stated that it is a fairly simple 6-lot subdivision with four lots on the road and two rear lots.

QUESTIONS/COMMENTS FROM COMMISSION MEMBERS/STAFF:

M. Sigfridson asked about the proposed access easement in favor of Lot 5 which appears to cross over the corner of Lot 5.

Mr. Archer explained that that is the RA/R-30 Zone line which has been changed. Ms. Sigfridson asked about the southerly line between Lots 5 and 6.

Mr. Archer explained that it is going to be just before the stone wall and he explained that Mr. Weaver is giving Lot 6 his son who will have full control over the stone wall. The access easement will be owned by Lot 6.

Ms. Roberson explained that both Lots 5 and 6 will be served by the accessway, but Lot 6 (the rearmost lot) owns the accessway. Mr. Archer agreed.

There was discussion regarding the property boundaries not following the stone wall. Ms. Roberson explained that none of the property boundaries are coincident with stone walls with the exception of the stone walls along Day Street. If the proper width (25 feet off the centerline of Day Street) is granted to the Town, that stone wall would be Town property. An aerial photo showing stone walls was displayed and Ms. Roberson explained and orientated the property. Since the stone walls are not protected by property boundaries, she explained that a restrictive covenant could be applied.

Ms. Roberson asked if utilities were proposed to be underground.

Mr. Weaver explained that the four lots in the R-30 Zone are going to be close enough to the road where they could utilize the powerlines going by instead of adding another telephone pole as numberous poles always get hit on Day Street. Lots 5 and 6 would have underground utilities going back to them. So they would only be adding one more pole to the mix. He feels this would be safer. Ms. Roberson stated that it is a preference based on the Subdivision Regulations.

Regarding storm water runoff, Ms. Roberson displayed the Development Plan and explained that, currently, the area is completely treed and she spoke of her concern regarding what the treeline would become and of its absorbant value. She feels that storm water runoff will shed off the property at a much faster rate and she suggested that the Commission could ask the Applicant to have the engineer prepare drainage calculations. If the engineer found that corrective measures were needed, they would design them. She said that sometimes raising the water level of the wetland has an impact on it. Ms. Roberson feels that there is potential for an issue and that it could be further reviewed by Mr. Pauley. Mr. Archer stated that if Syl Pauley, Town Engineer, felt that it was a concern, it would have been on his Engineer's review list.

Richard Ives, asked if the IWWC would have asked those questions.

Ms. Roberson explained that they look at impacts to wetlands, but they don't look at flooding on a road which is more of a PZC concern.

David Held, Provost & Rovero, stated that he understands Ms. Roberson's concern with the change in cover types, however, he explained that given the vast size of the wetland (which extends a good distance to the south beyond this proposal, and it is essentially flat) he feels that any increase in water level there from this development would be miniscule at best. He explained that given the overall size of the watershed in that large wetland area, which essentially functions as a detention basin, there would not be any means for impact there with water level. He commented that if there is an existing problem with flooding on Day Street, it would be beyond the scope of this six-lot subdivision. The solution would be more of a Town responsibility. Ms. Roberson stated that she appreciates Mr. Held's comments and she explained that the flow always goes to the south, it goes around the big field and connects with the Longbrook watershed, which eventually reaches the Quinebaug River.

Mr. Archer commented that Mr. Weaver (with the last subdivision that he did) had dedicated an easement to the Town in order to put the water from the road back into that wetland. Ms. Roberson stated agreement.

Ms. Roberson displayed, and paraphrased from, the letter from the Conservation Commission (dated July 7, 2020) in which they recommend open space dedication of some sort in an undeveloped portion of the property. Ms. Roberson explained the options for the PZC to consider. There was discussion regarding the amount of land to be considered if open space were chosen (15 percent of 7.52 acres or of 60.70 acres). Ms. Roberson offered to consult with Land Use Attorney, Peter Alter.

Mr. Ives stated that he had attended the Conservation Commission meeting and that it is their understanding that it is the subdivided portion.

Ms. Roberson clarified that that was the area that was appraised (7.5 acres). She explained that the remaining land would be unbuildable until the time when the remaining land is to be developed, then there would be an open space dedication.

Ms. Sigfridson asked that this be noted on the plans so that it will be clear in the future when someone comes before the Commission to subdivide the remaining 52 acres. Ms. Roberson commented that it could be written into a motion that it be added to the final subdivision plans that further development will require subdivision open space dedication. Mr. Archer stated that Mr. Weaver is fully aware that if he ever goes to develop the remaining land, it would have to come before the PZC for a subdivision. Ms. Sigfridson stated agreement.

Mr. Ives left the meeting at 8:45 p.m.

A. Fitzgerald stated that he would prefer open space to fee-in-lieu. There was discussion about this option.

C. Kelleher commented that it does not appear to be contiguous with anything that is already open space and she is not sure that this would be the best way to preserve land in that area. She prefers fee-in-lieu considering the size of land that would be involved in open space on this particular part of the property.

M. Sigfridson commented that she doesn't have a strong opinion either way.

E. Starks prefers fee-in-lieu.

A. Tanner prefers fee-in-lieu in this case.

Mr. Archer commented that Mr. Weaver had given almost \$10,000 fee-in-lieu for the 10-lot subdivision next door and adding \$6,000 for this subdivision, and he suggested that the Town could buy a piece of land for open space with the \$16,000.

Ms. Roberson drafted language for the open space dedication as follows: "At such point in the future the remaining land (53.4 acres) is proposed for development, it shall be required to be subdivided and it shall be subject to open space dedication."

Mr. Archer confirmed that the remaining land would be 53.4 acres.

There was discussion regarding the neighboring land owners not being notified. J. Roberson explained Section 4.2.10 of the Subdivision Regulations (page 13). The Executive Order from the Governor may cover both the Statutes and Regulations regarding noticing and publishing. It has been posted on the website. Ms. Sigfridson noted that the Applicant does not seem concerned about going forward with the Application regarding the possibility of an abutting land owner challenging the decision, if approved.

There was discussion regarding width dedication of the road.

Mr. Archer stated that he had spoken with Mr. Ives about it earlier in the day and that Mr. Ives had expressed that if it was only 1-2 feet, he doesn't want it. However, if the PZC feels it is necessary, Mr. Archer stated that Mr. Weaver has no problem dedicating that land to the Town (25 feet off the center line) and they would revise the plans. Ms. Sigfridson asked if the PZC has a choice in this matter.

Ms. Roberson explained that it is standard. She thinks it is more like five feet in some areas.

Mr. Archer suggested that instead of deeding over the land, they could grant an easement to the Town, so for Highway purposes, they would have the right to do what they need to do on the land. He said he has done this in other towns.

Due to development being on a Town road, Ms. Roberson read aloud from Section 1.4 of the Public Improvement Specifications (last updated 2007) which are referenced by the Subdivision Regulations: roadway improvements; street curbs; sidewalks; storm drainage; sediment basins. She will distribute copies of this Ordinance to Commission Members. There was discussion regarding existing vs. proposed roads. Ms. Roberson stated that, typically, the Town just gets the land dedication (Section 6.1 of the Subdivision Regulations).

There was discussion regarding the requirement for a Site Analysis Plan. Mr. Archer explained that it was an oversight on his part and he asked that a waiver be granted. He also offered that, if the Commissioner wants a Site Analysis Plan, it could be a condition of approval that one be submitted.

Mr. Tanner stated that he would prefer that one be submitted and agreed that it could be a condition of approval. Ms. Sigfridson stated agreement.

There was more discussion about the road dedication. Mr. Tanner noted that it is unclear because it is two feet in some places and five feet in other places. Mr. Archer explained that if the land is dedicated to the Town, then the Town would own all of the stone walls along with the concern of rebuilding them. Mr. Tanner noted that if the Town owns them, then nobody would be selling them. Mr. Archer questionned whether the Applicant has the right to rebuild the stone wall once it is broken through, if it has been deeded to the Town. Mr. Tanner stated agreement with Mr. Archer's comment that he would offset the centerline of the road whether it is a foot, five feet or six inches and deed that land over to the Town.

Ms. Roberson commented that anything in Section 5 Section – Section 12.6 of the Subdivision Regulations (where the Site Analysis Plan is listed) may be waived by the Commission with a written note from the Applicant and a ³/₄ vote of the Commission. Mr. Tanner stated that he doesn't care one way or the other. Ms. Kelleher and Ms. Sigfridson both stated agreement.

Motion was made by C. Kelleher to waive the requirement for a Site Analysis Plan for **SD 20-001** – 6-Lot Subdivision, Applicant: Jeff Weaver, 8 acres on south side of Day St., (Map 43, Lot 6) in the RA and R-30 Zones; Proposed creation of six residential lots. Second by E. Starks. No discussion. Roll Call Vote: C. Kelleher – yes; A. Tanner – yes; E. Starks – yes; A. Fitzgerald – yes; M. Sigfridson – yes. Motion carried unanimously (5-0-0).

Motion was made by E. Starks to approve the application SD 20-001 – 6-Lot Subdivision, Applicant: Jeff Weaver, 8 acres on south side of Day St., (Map 43, Lot 6) in the RA and R-30 Zones; Proposed creation of six residential lots and remaining land in the R-30 and RA Zones in accordance with all final plans, documents and testimony submitted with the application and including the following conditions:

At such point in the future that the remaining land (50+/- as it may change a little due to the dedication of the road) is proposed for development, it shall be required to be subdivided and it shall be subject to open space dedication.

The following items shall be corrected on the Final Subdivision Plans which will be submitted in paper for checking to the Land Use Office prior to being printed on archival material (mylar):

• A land dedication to the Town of Brooklyn shall be added along Day Street for the purposes of meeting the Public Improvement Specifications.

Prior to endorsement by the Commission of the Final Subdivision Plan(s) for filing in the office of the Town Clerk:

- The approval and/or review letters from the Inland Wetlands and Watercourses Commission, the Northeast District Department of Health, and the Planning and Zoning Commission shall be added to the Final Subdivision Plan(s).
- A Shared Driveway and Maintenance Agreement for the shared driveway in a form acceptable to the Town Attorney shall be filed simultaneously with the recording of the subdivision mylars in the office of the Town Clerk.
- All boundary pins and monuments shall be set and field verified by the surveyor.

At the time of sale of any building lot:

• At the time of sale of any of the six new building lots, a payment in lieu of open space dedication shall be paid by the applicant to the Town in the amount of \$1,000 per lot in accordance with the requirements of CT General Statutes 8-25 and Brooklyn Subdivision Regulation Sec. 8. An open space lien may be placed on the building lots to ensure that the fee-in-lieu of open space is paid at the time of sale.

Prior to the issuance of a Zoning Permit on any lot:

- The developer shall notify the Zoning Enforcement Office and Town Planner at least seven days in advance of any site work to schedule a pre-construction meeting.
- Prior to any lots being developed, driveway permits must be obtained from the Road Foreman in accordance with the adopted policy concerning driveways. No stonewalls, mature trees, or ledge within the r.o.w. shall be removed or modified unless necessary for safety. The responsibility of clearing, grubbing, blasting, and earthmoving with the Town of Brooklyn r.o.w. shall be the responsibility of the individual lot developer. Any cutting of trees greater than 30" d.b.h. for sightlines shall require prior approval by the Town of Brooklyn Tree Warden upon finding that the removal of trees is unavoidable to guarantee adequate driveway sightlines.
- The applicant and/or individual lot developers shall minimize impacts to natural features both on private lots and in the Town of Brooklyn r.o.w. to the greatest extent possible. This shall include but is not limited to the preservation of stonewalls, the protection of mature trees lining any public road, and the minimization of clearing and grading.
- Stonewalls must be finished on the edges prior to the issuance of a Certificate of Zoning Compliance on any lot containing a stone wall.

Second by C. Kelleher. No discussion.

Roll Call Vote: A. Tanner – yes; E. Starks – yes; A. Fitzgerald – yes; C. Kelleher – yes; M. Sigfridson – yes. Motion carried unanimously (5-0-0).

3. **ZRC 20-001 rev** – A proposal to make amendments to the Zoning Regulations concerning accessory buildings, excavation operations, and other various corrections including Sec. 3.A.5.2.1, 3.B.5.2.1, 3.C.5.2.1, 4.A.4.2.1, 4.B.4.2.1, 4.C.4.2.1, 3.C.2.4.5, 4.D.2.1.5, 6.K.2.2, 6.O.4.1, 6.P.3.3, 7.B.5.4.

Motion was made by E. Starks to approve the proposal to make the following amendments to the Zoning Regulations concerning accessory buildings, excavation operations, and other various corrections including Sec. 3.A.5.2.1, 3.B.5.2.1, 3.C.5.2.1, 4.A.4.2.1, 4.B.4.2.1, 4.C.4.2.1, 3.C.2.4.5, 4.D.2.1.5, 6.K.2.2, 6.O.4.1, 6.P.3.3, 7.B.5.4 with the finding that they will aid in the protection of public health, safety, welfare and property values and are consistent with the Plan of Conservation and Development and the intent of the Zoning Regulations. The regulations shall become effective 15 days from the date of publication on the website. Second by A. Tanner.

There was discussion regarding containers:

• M. Sigfridson noted that her understanding from the earlier discussion is that the Commission will look further into the issue of containers at the next meeting of the PZC. Ms. Roberson noted that the other issue is the special permit. Ms. Kelleher stated it was her understanding that they did not want to make the change that would require a special permit. Ms. Sigfridson agreed that that was also her understanding and she stated that they just wanted to change the setbacks to match the setbacks in the zone for the primary structure. Discussion continued about how to do it: whether to amend the motion or to change the Zone Change Application. Ms. Roberson suggested changing the Zone Change Application to read as follows: "For all of the listed sections (that end with 2.1), the minimum front yard setback for accessory buildings shall be the same as for principle buildings in the zone." Mr. Tanner would like it to be stated that it would not require a public hearing.

Ms. Roberson explained that you can change the content of the proposal as long as the change was discussed when the public hearing was open. Ms. Sigfridson agreed. Discussion continued.

E. Starks amended his motion to approve the proposal pursuant to discussion of the Commission such that any reference to accessory buildings requiring a special permit would be replaced with accessory buildings requiring a setback equal to that of the principle structure. Second by A. Tanner. Discussion:

Ms. Sigfridson explained the motion so that the Commission Members were clear on what they were voting on.

Mr. Tanner feels that this is not parliamentary correct and suggested that the motion be withdrawn and restated.

Mr. Starks rescinded his motion. Mr. Tanner rescinded his second.

Motion was made by E. Starks to approve the proposal to make amendments to the Zoning Regulations as they appear in the Application of those amendments regarding Sections 3.A.5.2.1, 3.B.5.2.1, 3.C.5.2.1, 4.A.4.2.1, 4.B.4.2.1, 4.C.4.2.1 in as much as those amendments would be to allow accessory buildings in the front yard pursuant to the setbacks in that zone rather than by special permit. Second by A. Tanner. Discussion:

- Ms. Sigfridson explained that her understanding of the effect would be, as discussed, regarding the front yard accessory building issue, and for that, rather than allowing by special permit, they would just be allowed if they are within the setback provided for primary buildings. No opposition stated.
- There was discussion regarding whether this is a restated motion or a new motion. It was decided that it is a new motion.

Roll Call Vote: A. Fitzgerald – yes; C. Kelleher – yes; A. Tanner – yes; E. Starks – yes; M. Sigfridson – yes. Motion carried unanimously (5-0-0).

VII. New Business:

a. Applications:

1. **SPG 20-001** – Gravel Special Permit, Paul R. Lehto, 71.34 acres on the east side of Allen Hill Road (Map 32, Lot 148), in the RA Zone; Excavation of approximately 90,000 cubic yards of sand and gravel on 6.7 acres.

David Held, Provost & Rovero represented the Applicant. Mr. Held stated that he would give the presentation at the public hearing. He commented that if the public hearing were held on July 21st, it would not allow for the proper noticing period.

There was discussion regarding the Governor's Executive Order 7-I which allows for posting a sign on the property rather than sending certified letters to abutters.

Motion was made by A. Tanner to schedule a public hearing for **SPG 20-001** – Gravel Special Permit, Paul R. Lehto, 71.34 acres on the east side of Allen Hill Road (Map 32, Lot 148), in the RA Zone; Excavation of approximately 90,000 cubic yards of sand and gravel on 6.7 acres, to be held at a regular meeting of the Planning and Zoning Commission to be held on August 5, 2020 at 6:30 p.m., access via web and phone to be provided on the meeting agenda to be posted on the Town of Brooklyn, CT website. Second by C. Kelleher. No discussion.

Roll Call Vote: C. Kelleher – yes; A. Tanner – yes; E. Starks – yes; A. Fitzgerald – yes; M. Sigfridson – yes. Motion carried unanimously (5-0-0).

b. Other New Business: None.

 SPG 19-003 - *scrivener's correction* Gravel Special Permit, Strategic Commercial Realty, Inc, d/b/a Rawson Materials, 30 acres on south side of Maynard Road (Map 29, Lot 5) in the RA Zone; Excavation of approximately 1.05 million cubic yards of sand and gravel.

Ms. Roberson explained that there was an error in the original motion regarding the renewal date. The special permit has not been recorded yet.

Motion was made by C. Kelleher to correct item #10 from the approval of SPG 19-003 granted at the June 3, 2020 meeting of the Planning and Zoning Commission to specify that the permit renewal date is June 3, 2021. Second by A. Tanner. No discussion.

Roll Call Vote: A. Tanner – yes; E. Starks – yes; A. Fitzgerald – yes; C. Kelleher – yes; M. Sigfridson – yes. Motion carried unanimously (5-0-0).

 SPG 19-004 – *scrivener's correction* Gravel Special Permit, Strategic Commercial Realty, Inc, d/b/a Rawson Materials, 200 acres+ on the south side of Rukstela Road (Map 21, Lot 7; Map 30, Lot 16) in the RA Zone; Excavation of approximately 1.55 million cubic yards of sand and gravel.

Ms. Roberson explained that there was an error in the original motion regarding the renewal date. The special permit has not been recorded yet.

Motion was made by C. Kelleher to correct item #9 from the approval of SPG 19-004 granted at the June 3, 2020 meeting of the Planning and Zoning Commission to specify that the permit renewal date is June 3, 2021. Second by A. Tanner. No discussion.

Roll Call Vote: E. Starks – yes; A. Fitzgerald – yes; C. Kelleher – yes; A. Tanner – yes; M. Sigfridson – yes. Motion carried unanimously (5-0-0).

3. **SP 08-005** – **Modification #2 - *scrivener's correction*** Walmart, 450 Providence Road (Map 41, Lot 10) Re-striping of ten (10) wider pick-up stalls, new parking lot directional signs and pavement markings, new exterior wall sign.

Ms. Roberson explained that the grey coloring on the plan made it difficult to read and she was told the day after the public hearing that it was actually ten spaces and not nine as thought at the public hearing. She said that there are two rows of nine spaces.

Motion was made by C. Kelleher to correct the number of parking spaces from the approval of SP 08-005 mod 2 to specify that the number of new designated spaces to be added is ten (10). Second by E. Starks. No discussion.

Roll Call Vote: A. Fitzgerald – yes; C. Kelleher – yes; A. Tanner – yes; E. Starks – yes; M. Sigfridson – yes. Motion carried unanimously (5-0-0).

VIII. Reports of Officers and Committees:

a. Staff Reports

Margaret Washburn's Report (dated June 30, 2020) was included in packets to Commission Members. No discussion.

Ms. Roberson will ask Ms. Washburn when she would be available to attend a meeting.

- Budget Update (Included in packets to Commission Members)
 Ms. Roberson explained that the \$7,715 shown in Revenues is incorrect. She feels that it should be more like \$18,000. There was discussion regarding the discrepancy.
- c. Correspondence None.
- d. Chairman's Report

Ms. Sigfridson asked Ms. Roberson if Mr. Ives will be available to meet to discuss the POCD. Ms. Kelleher suggested a review of the Subdivision Regulations be done soon as they need to be revised.

IX. Public Commentary – None.

X. Adjourn

M. Sigfridson adjourned the meeting at 10:26 p.m.

Respectfully submitted, J.S. Perreault Recording Secretary Brooklyn Planning & Zoning Commission Special Meeting - Webex, Wednesday, July 8, 2020

4.2			OFFICE OF SELECTMEN TELEPHONE: 779-3411
			TOWN CLERK TELEPHONE: 774-9543
- 11 - 3	TOWN OF BI		ASSESSOR TELEPHONE: 774-5611
5	P.O. Box 356 - Rou BROOKLYN, CONNE		TAX COLLECTOR TELEPHONE: 774-4072
RECEIVED		011001 00204	JUDGE OF PROBATE TELEPHONE: 774-5973
JUN 0 3 2020			TELEFOONE. 774-5973
Received Date	a	Applica	tion #SPG <u>20-00(</u>
Fee \$ 250 State Fee (\$	0.00)		Check # 6129
A DD	LICATION FOR	CDAVEL DAND	7
ALL	SPECIAL P		A.
	of Boline II		
Name of Applicant Paul R. Lehto		Pho	ne 860-208-9789
Mailing Address 40 Almada Drive, Broc	klyn, CT 06234		
Relation owner			2004 Contraction (1997)
Property Owner Paul R. Lehto		Dh	ne 860-208-9789
Mailing Address 40 Almada Drive, Broo	klyn, CT 06234	Fill	Me_000-200-5703
			and the second
Name of Engineer/Surveyor Provost & Address P.O. Box 191, Plainfield, CT 00	Rovero, Inc.		
Address P.O. Box 191, Plainfield, CT 06	5374		
Contact Person David J. Held, P.E., L.	5.	Phone 860-230-0856 F	ax
Name of Attorney_N/A		8	
Address			
Phone Fax			and the second
Property address	en Hill Road	(D)	
Property Location		CRivervolk I	Jrive)
Map #_32 Lot #148 Zon	B RA Total Acres	71.34	
	5 I Oldi Acies		
Maximum Area :			
Acres of Gravel Removal 6.7 G	Cres Cubic Ya	rds of Gravel Removal	90,000 CF
Is Application for Renewal? Yes	No X If	Vec Amount Removed (act Voor
Original Date of Issuance of Permit		Issued To:	ast Year
Open line of the ball of the second			
Compliance with Article 13, Gravel			
Compliance with <u>Article 5</u> , Special	Permit Requirements		
The owner and applicant hereby gran	the Brooklyn Planning of	ad Zoning Commission	he Deerd of Calastron
Authorized Agents of the Planning an	d Zoning Commission or	Roard of Selectman ner	ne board of Selectman,

which the application is requested for the purpose of inspection and enforcement of the Zoning regulations and the Subdivision regulations of the Town of Brooklyn

Date 5 20 2020 Date 5 /20/2020 Applicant: an ento Owner:
 Owner:
 Towl
 Lehno

 *Note :
 All consulting fees shall be paid by the applicant

2

EARTH EXCVATION AND REMOVAL CHECK LIST

The following items are required as a part of the excavation plan. Note these are minimum requirements. Other information may be required based on your application

X Contours at 2' intervals

.

For renewals:

Contours as of original permit approval

-----Contours as of date of survey(updated to present) stamped by a licensed land SURVEYOR

X___ Amount of material to be removed

For Renewals:

Amount of material originally approved to be removed

- ----- Amount of material removed to date, by an annual accounting for each 12 month period of the permit
- Amount of material to be removed during the next year
- ____ Date the permit will next expire if not renewed.
- $\frac{X}{X}$ Maximum depth of excavation Depths to water table
- _ Depths to water table
- X Note measures to be used to protect the water table
- Х Location of any stock piles
- X Areas to be restored
- X Restoration Plan
- X Erosion and Sediment Control Plan
- Erosion and Sediment Control Narrative
- X Erosion and Sediment Control Bond For renewals:

 - Amount of bond that has been filed
 - Verification of Erosion and Sedimentation control measures
- X Traffic pattern within the site
- Will any trucks be repaired on site if so, where
- X Location of fueling pad
- Will any equipment or trucks be stored on site
 - If so, locate on site
- X Average number of trips per day
- X Maximum number of trips per day
- X Note trucks will be covered when leaving the site

- _____ Processing equipment if any and usage
 - Amount of processing too be done

_____ Per year

Per month

- How will noise issues be addressed
- X How will dust issues be address
 - X Calcium chloride X water at what frequency
- X Description of the project, trucks/day, days and hours of operation, completion date etc
- X Phasing plan

4

- ____ Time frame for project
- ____Site inspection by staff
- _____ Compliance with Article 5 Special Permit
- Compliance with Article13 Gravel Banks
- For Renewals:
- _____ Inland Wetlands Permit if required
- Archeological review
- DEP Permit if required

Other items to review

Bond amount may need to be updated regarding the following:

Erosion and Sediment Control
Restoration Plan

Inspections will be done through out the year on a Quarterly basis to insure compliance with the original plan and any conditions of renewal

Civil Engineering

Surveying •

Site Planning

Structural

Mechanical

P.O. Box 191 57 East Main Street Plainfield, CT 06374 Architectural Engineering

Telephone (860) 230-0856 Fax (860) 230-0860 www.prorovinc.com

June 2, 2020

Brooklyn Planning & Zoning Commission 69 South Main Street Brooklyn, CT 06234

RE: Paul R. Lehto – Proposed Gravel Excavation – Easterly of Allen Hill Road – Brooklyn, CT P&R Job No. 173055

Dear Commissioners:

This narrative is intended to accompany the special permit application for the proposed gravel excavation by Paul R. Lehto. The proposed excavation site is an extension of a previously permitted excavation and will include 6.7 acres and result in the removal of approximately 90,000 cubic yards of material. An application for this project has also been submitted to the Brooklyn Inland Wetlands & Watercourses Commission.

The zoning regulations require an excavation permittee to provide a bond for restoration of the site following excavation activities. As noted above, the subject property was previously permitted for excavation in an area immediately adjacent to the currently proposed excavation site. The Town is currently in possession of the cash bond which was required as part of that previously approved excavation. The current excavation site encompasses 6.7 acres of new site disturbance. We would propose a restoration bond amount of \$10,000.00 per acre or \$67,000.00 for the current proposal. If the applicant wishes to bond by phase, the first excavation phase includes 4.1 acres of disturbance with a resulting bond amount of \$41,000.00 This amount would cover grading the excavation area in accordance with the zoning regulations (2H:1V maximum slopes), spreading on-site stockpiled topsoil and seeding with an appropriate seed mix. For informational purposes, we have included a conceptual subdivision plan as part of this application to demonstrate the feasible reuse of the property following excavation and restoration.

Thank you for your consideration of this application. If you have any questions or need additional information, please do not hesitate to contact us at your convenience.

Sincerely,

David J. Held, P.E., L.S. Provost & Rovero, Inc.

٠

Surveying

Civil Engineering

P.O. Box 191 57 East Main Street Plainfield, CT 06374 .

Telephone (860) 230-0856 Fax (860) 230-0860 www.prorovinc.com

June 2, 2020

Paul R. Lehto

P & R Job #173055

APPLICATION PACKAGE CONTENTS – Excavation Special Permit Application

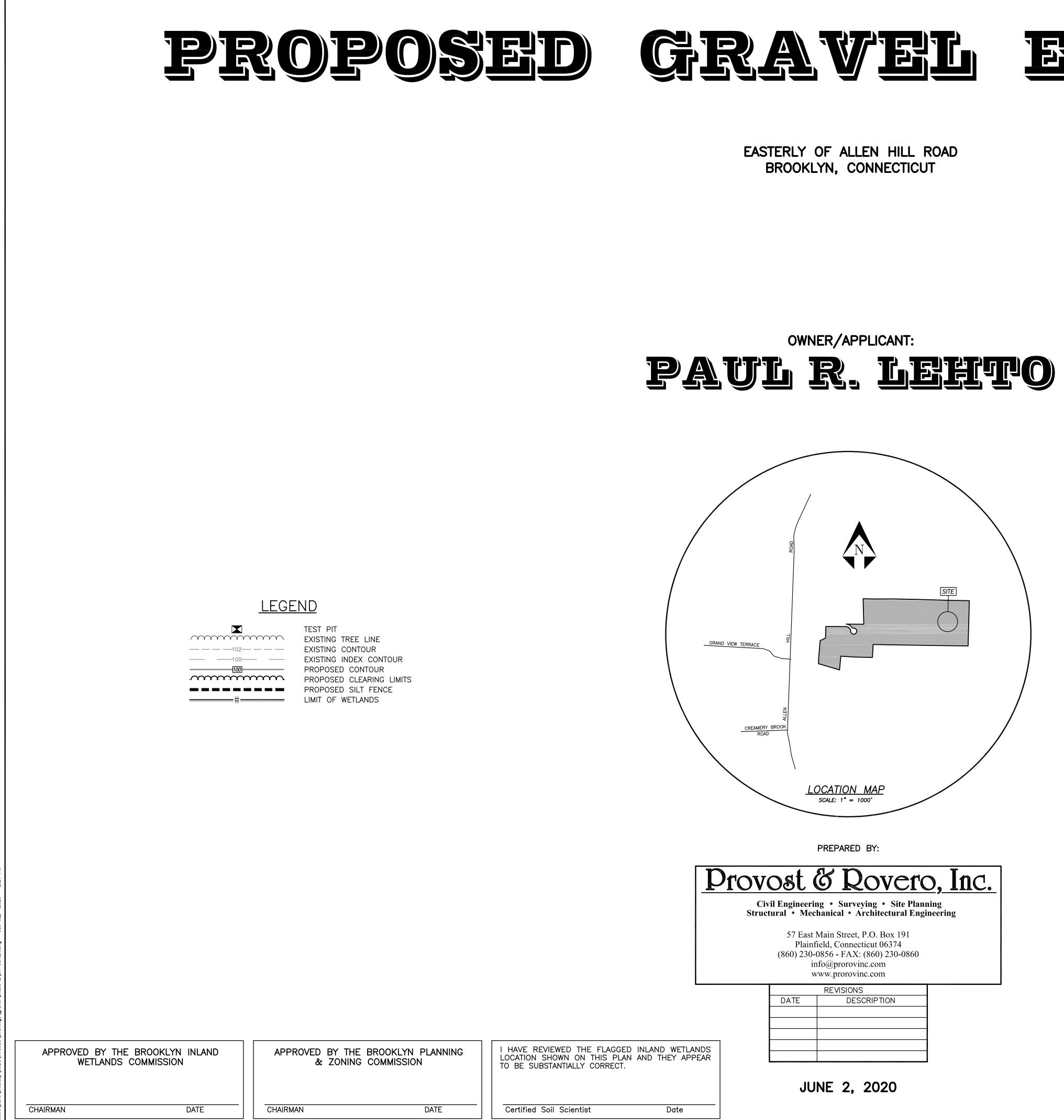
Site Planning

•

- 1. Application fee \$3,110.00
- 2. Special permit application form
- 3. Application narrative dated 6/2/2020
- 4. 5 copies of excavation plans dated 6/2/2020

Application Fee Calculation:

Base Fee:	\$	250.00
State Fee:	\$	60.00
Public Hearing Fee:	\$	300.00
50,000 CY-100,000 CY Fee:	\$2	2,500.00
TOTAL FEE:	\$3	3,110.00





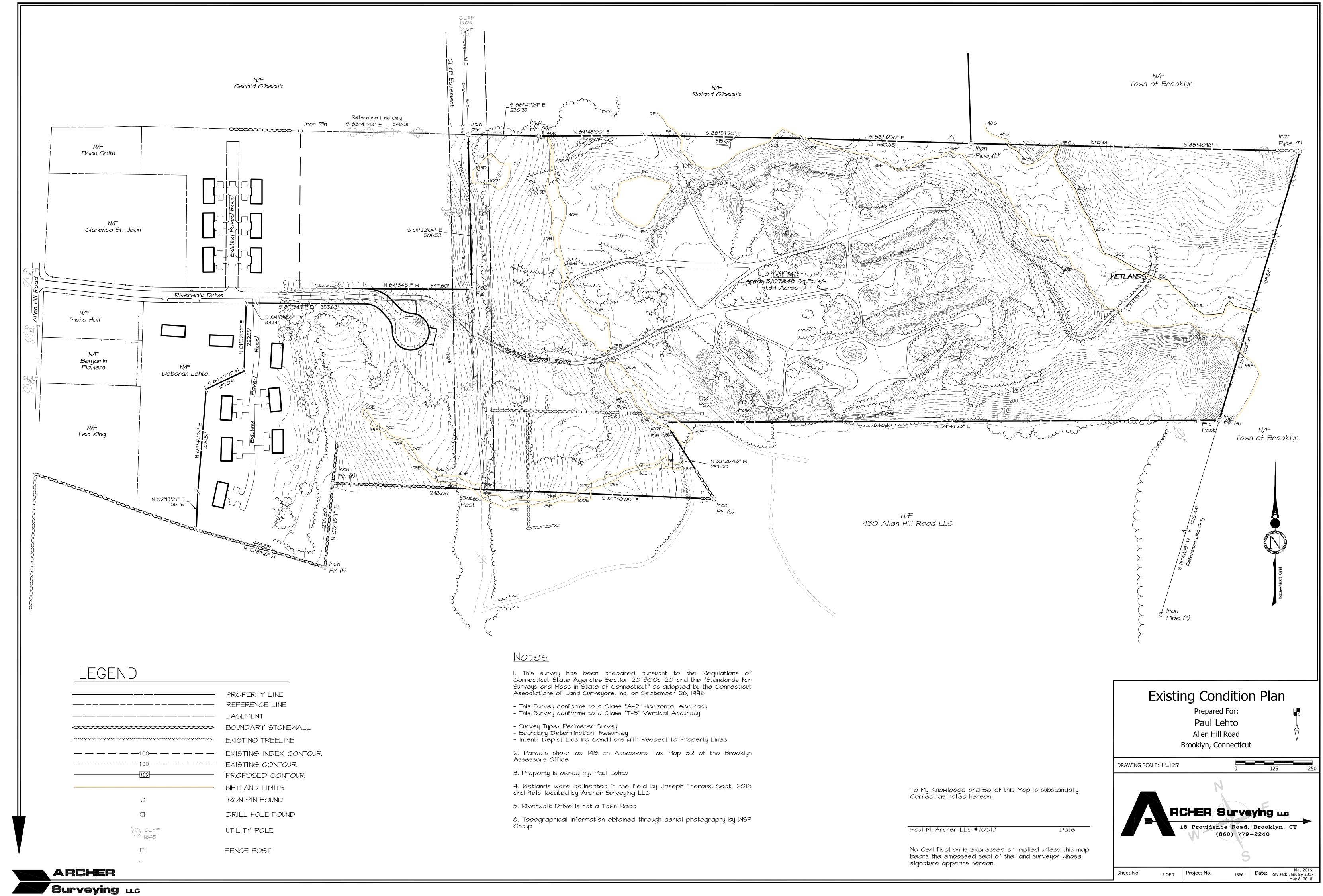
INDEX TO DRAWINGS

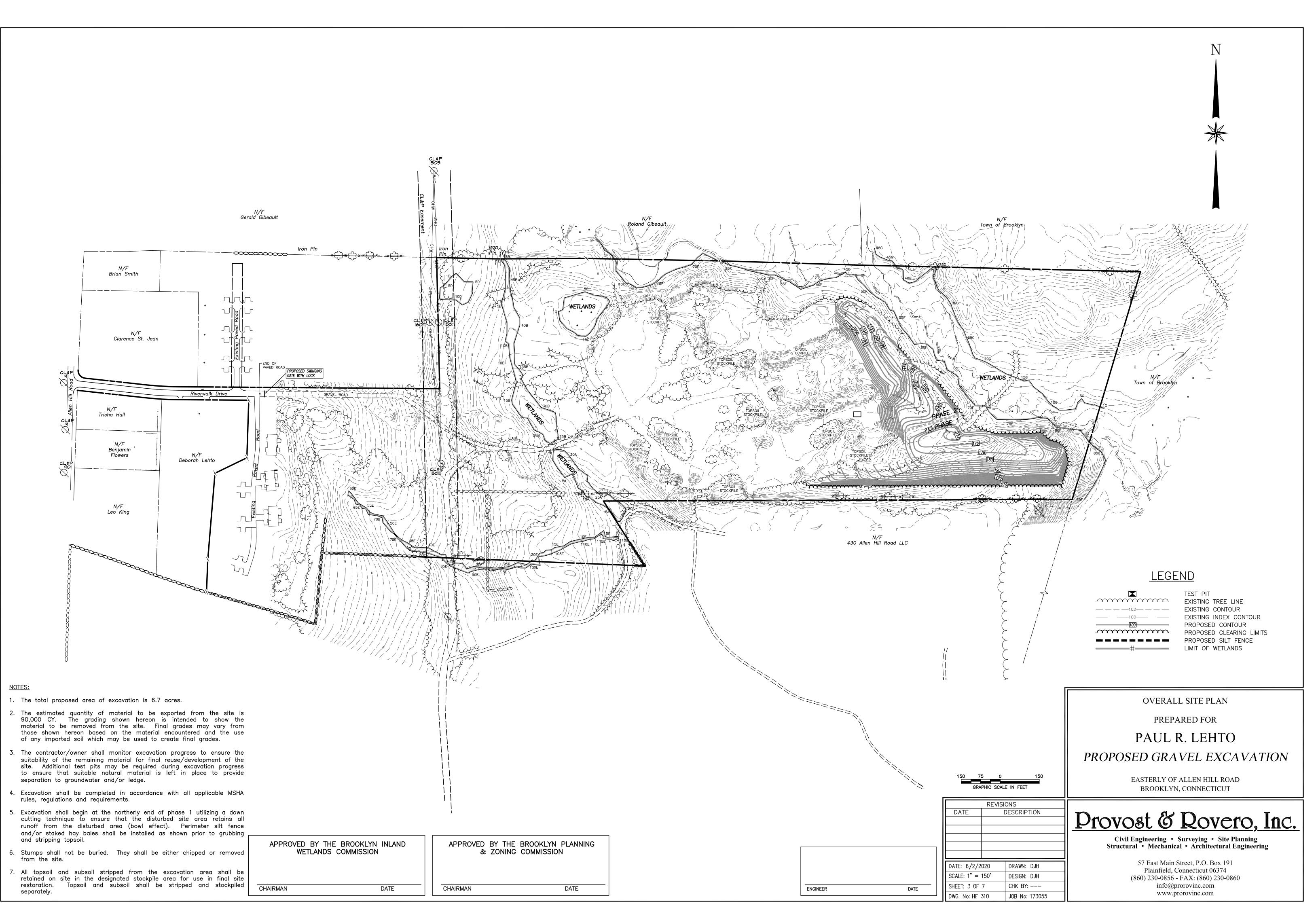
TITLE	SHEET No.
COVER SHEET	1 OF 7
EXISTING CONDITIONS PLAN	2 OF 7
OVERALL SITE PLAN	3 OF 7
PROPOSED EXCAVATION PLAN	4 OF 7
DETAIL SHEET	5 OF 7
SITE REUSE PLAN	6 OF 7
SITE RADIUS PLAN	7 OF 7

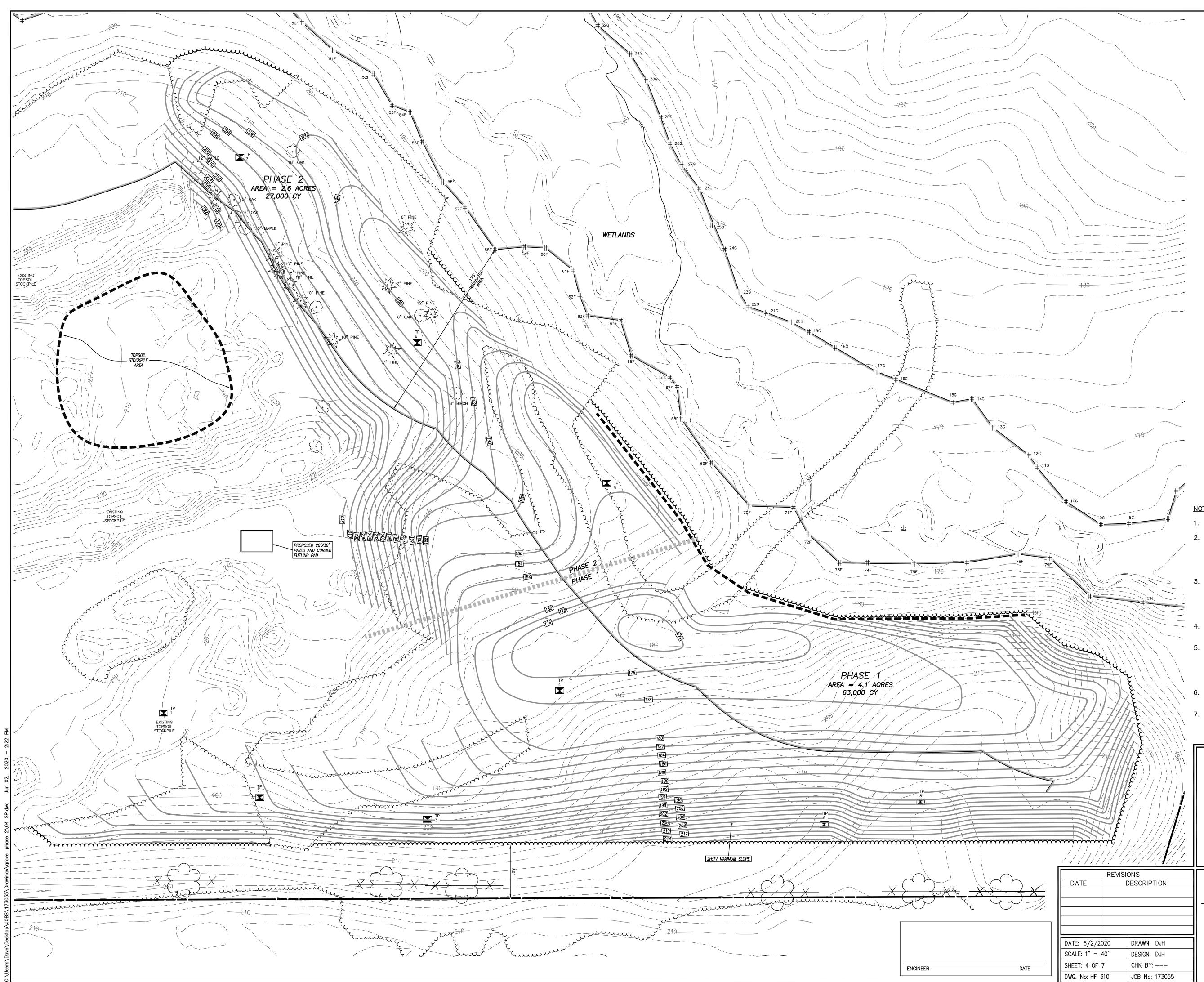
ENGINEER

DATE

SHEET 1 OF 7 JOB NO: 173055 DWG NO: HF 310







	40 20 0 40 GRAPHIC SCALE IN FEET
CHAIRMAN	DATE
APPROVED BY THE & ZONING	BROOKLYN PLANNING COMMISSION
CHAIRMAN	DATE
LEGE	ND
	TEST PIT EXISTING TREE LINE EXISTING CONTOUR EXISTING INDEX CONTOUR PROPOSED CONTOUR

NOTES:

- 1. The total proposed area of excavation is 6.7 acres.
- 2. The estimated quantity of material to be exported from the site is 90,000 CY. The grading shown hereon is intended to show the material to be removed from the site. Final grades may vary from those shown hereon based on the material encountered and the use of any imported soil which may be used to create final grades.
- 3. The contractor/owner shall monitor excavation progress to ensure the suitability of the remaining material for final reuse/development of the site. Additional test pits may be required during excavation progress to ensure that suitable natural material is left in place to provide separation to groundwater and/or ledge.
- 4. Excavation shall be completed in accordance with all applicable MSHA rules, regulations and requirements.
- 5. Excavation shall begin at the northerly end of phase 1 utilizing a down cutting technique to ensure that the disturbed site area retains all runoff from the disturbed area (bowl effect). Perimeter silt fence and/or staked hay bales shall be installed as shown prior to grubbing and stripping topsoil.
- 6. Stumps shall not be buried. They shall be either chipped or removed from the site.
- All topsoil and subsoil stripped from the excavation area shall be retained on site in the designated stockpile area for use in final site restoration. Topsoil and subsoil shall be stripped and stockpiled separately.

PROPOSED EXCAVATION PLAN

PREPARED FOR

PAUL R. LEHTO

PROPOSED GRAVEL EXCAVATION

EASTERLY OF ALLEN HILL ROAD BROOKLYN, CONNECTICUT

Provost & Rovero, Inc.

Civil Engineering • Surveying • Site Planning Structural • Mechanical • Architectural Engineering

> 57 East Main Street, P.O. Box 191 Plainfield, Connecticut 06374 (860) 230-0856 - FAX: (860) 230-0860 info@prorovinc.com www.prorovinc.com

EROSION AND SEDIMENT CONTROL PLAN:

REFERENCE IS MADE TO:

- 1. Connecticut Guidelines for Soil Erosion and Sediment Control 2002 (2002 Guidelines).
- 2. Soil Survey of Connecticut. N.R.C.S.
- SILT FENCE INSTALLATION AND MAINTENANCE:
- 1. Dig a 6" deep trench on the uphill side of the barrier location.
- 2. Position the posts on the downhill side of the barrier and drive the posts 1.5 feet into the around.
- 3. Lay the bottom 6" of the fabric in the trench to prevent undermining and backfill.
- 4. Inspect and repair barrier after heavy rainfall.
- 5. Inspections will be made at least once per week and within 24 hours of the end of a storm with a rainfall amount of 0.5 inch or greater to determine maintenance needs.
- 6. Sediment deposits are to be removed when they reach a height of 1 foot behind the barrier or half the height of the barrier and are to be deposited in an area which is not regulated by the inland wetlands commission.
- 7. Replace or repair the fence within 24 hours of observed failure. Failure of the fence has occurred when sediment fails to be retained by the fence because: - the fence has been overtopped, undercut or bypassed by runoff water.
- the fence has been moved out of position (knocked over), or
- the geotextile has decomposed or been damaged.

HAY BALE INSTALLATION AND MAINTENANCE:

- 1. Bales shall be placed as shown on the plans with the ends of the bales tightly abutting each other
- 2. Each bale shall be securely anchored with at least 2 stakes and apps between bales shall be wedged with straw to prevent water from passing between the bales.
- 3. Inspect bales at least once per week and within 24 hours of the end of a storm with a rainfall amount of 0.5 inches or greater to determine maintenance needs.
- 4. Remove sediment behind the bales when it reaches half the height of the bale and deposit in an area which is not regulated by the Inland Wetlands Commission.
- 5. Replace or repair the barrier within 24 hours of observed failure. Failure of the barrier has occurred when sediment fails to be retained by the barrier because: - the barrier has been overtopped, undercut or bypassed by runoff water,
- the barrier has been moved out of position, or
- the hay bales have deteriorated or been damaged.

TEMPORARY VEGETATIVE COVER:

SEED SELECTION

Grass species shall be appropriate for the season and site conditions. Appropriate species are outlined in Figure TS-2 in the 2002 Guidelines.

TIMING CONSIDERATIONS

Seed with a temporary seed mixture within 7 days after the suspension of grading work in disturbed areas where the suspension of work is expected to be more than 30 days but less than 1 vear.

SITE PREPARATION

Install needed erosion control measures such as diversions, grade stabilization structures, sediment basins and grassed waterways.

Grade according to plans and allow for the use of appropriate equipment for seedbed preparation, seeding, mulch application, and mulch anchoring.

SEEDBED PREPARATION

Loosen the soil to a depth of 3-4 inches with a slightly roughened surface. If the area has been recently loosened or disturbed, no further roughening is required. Soil preparation can be accomplished by tracking with a bulldozer, discing, harrowing, raking or dragging with a section of chain link fence. Avoid excessive compaction of the surface by equipment traveling back and forth over the surface. If the slope is tracked, the cleat marks shall be perpendicular the anticipated direction of the flow of surface water.

If soil testing is not practical or feasible on small or variable sites, or where timing is critical, fertilizer may be applied at the rate of 300 pounds per acre or 7.5 pounds per 1,000 square feet of 10-10-10 or equivalent. Additionally, lime may be applied using rates given in Figure TS-1 in the 2002 Guidelines.

SEEDING

Apply seed uniformly by hand cyclone seeder, drill, cultipacker type seeder or hydroseeder at a minimum rate for the selected species. Increase seeding rates by 10% when hydroseeding. MULCHING

Temporary seedings made during optimum seeding dates shall be mulched according to the recommendations in the 2002 Guidelines. When seeding outside of the recommended dates, increase the application of mulch to provide 95%-100% coverage.

MAINTENANCE

Inspect seeded area at least once a week and within 24 hours of the end of a storm with a rainfall amount of 0.5 inch or greater for seed and mulch movement and rill erosion.

Where seed has moved or where soil erosion has occurred, determine the cause of the failure. Repair eroded areas and install additional controls if required to prevent reoccurrence of erosion

Continue inspections until the grasses are firmly established. Grasses shall not be considered established until a ground cover is achieved which is mature enough to control soil erosion and to survive severe weather conditions (approximately 80% vegetative cover). PERMANENT VEGETATIVE COVER:

Refer to Permanent Seeding Measure in the 2002 Guidelines for specific applications and details related to the installation and maintenance of a permanent vegetative cover. In general, the following sequence of operations shall apply:

- Topsoil will be replaced once the excavation and grading has been completed. Topsoil will be spread at a minimum compacted depth of 4".
- 2. Once the topsoil has been spread, all stones 2" or larger in any dimension will be removed as well as debris.
- Apply agricultural ground limestone at a rate of 2 tons per acre or 100 lbs. per 1000 s.f. Apply 10-10-10 fertilizer or equivalent at a rate of 300 lbs. per acre or 7.5 lbs. per 1000 s.f. Work lime and fertilizer into the soil to a depth of 4".
- 4. Inspect seedbed before seeding. If traffic has compacted the soil, retill compacted areas.
- 5. Apply the chosen grass seed mix. The recommended seeding dates are: April 1 to June 15 & August 15 - October 1.
- 6. Following seeding, firm seedbed with a roller. Mulch immediately following seeding. If a permanent vegetative stand cannot be established by September 30, apply a temporary cover on the topsoil such as netting, mat or organic mulch.

EROSION AND SEDIMENT CONTROL NARRATIVE:

PRINCIPLES OF EROSION AND SEDIMENT CONTROL

The primary function of erosion and sediment controls is to absorb erosional energies and reduce runoff velocities that force the detachment and transport of soil and/or encourage the deposition of eroded soil particles before they reach any sensitive area.

KEEP LAND DISTURBANCE TO A MINIMUM

The more land that is in vegetative cover, the more surface water will infiltrate into the soil, thus minimizing stormwater runoff and potential erosion. Keeping land disturbance to a minimum not only involves minimizing the extent of exposure at any one time, but also the duration of exposure. Phasing, sequencing and construction scheduling are interrelated. Phasing divides a large project into distinct sections where construction work over a specific area occurs over distinct periods of time and each phase is not dependent upon a subsequent

phase in order to be functional. A sequence is the order in which c to occur during any particular phase. A sequence should be develo first things first" and "last things last" with proper attention given adequate erosion and sediment control measures. A construction sche time lines applied to it and should address the potential overlap of which may be in conflict with each other.

- Limit areas of clearing and grading. Protect natural vegetation from with fencing, tree armoring, and retaining walls or tree wells.
- Route traffic patterns within the site to avoid existing or newly plante - Phase construction so that areas which are actively being develope
- minimized and only that area under construction is exposed. essential for construction.
- Sequence the construction of storm drainage systems so that they as possible during construction. Ensure all outlets are stable drainage flow into them.
- Schedule construction so that final grading and stabilization is possible

SLOW THE FLOW

Detachment and transport of eroded soil must be kept to a minir reducing the erosive energy of water. The erosive energy of water ir and velocity of runoff increases. The volume and velocity of development as a result of reduced infiltration rates caused by t vegetation, removal of topsoil, compaction of soil and the construction of

- Use diversions, stone dikes, silt fences and similar measures to dissipate storm water energy.
- Avoid diverting one drainage system into another without calculo downstream flooding or erosion.

KEEP CLEAN RUNOFE SEPARATED

Clean runoff should be kept separated from sediment laden water and over disturbed areas without additional controls. Additionally, preven off-site generated runoff with sediment laden runoff generated on-si filtration of on-site waters has occurred.

- Segregate construction waters from clean water.
- Divert site runoff to keep it isolated from wetlands, watercourses flow through or near the development until the sediment in that detained.
- REDUCE ON SITE POTENTIAL INTERNALLY AND INSTALL PERIMETER CONTROL

While it may seem less complicated to collect all waters to one treatment and just install a perimeter control, it can be more eff controls to many small sub-drainage basins within the site. By refrom within the site, the chance of perimeter control failure and the po that it can cause is reduced. It is generally more expensive to correct it is to install proper internal controls.

- Control erosion and sedimentation in the smallest drainage area po control erosion than to contend with sediment after it has been deposited in unwanted areas.
- Direct runoff from small disturbed areas to adjoining undisturbed veg the potential for concentrated flows and increase settlement and filter
- Concentrated runoff from development should be safely conveyed to rapped channels, waterways, diversions, storm drains or similar measure
- Determine the need for sediment basins. Sediment basins o developments where major grading is planned and where it is impo control erosion at the source. Sediment basins are needed on larc sensitive areas such as wetlands, watercourses, and streets would I sediment deposition. Do not locate sediment basins in wetla intermittent watercourses. Sediment basins should be located to inter entry into the wetland or watercourse.
- Grade and landscape around buildings and septic systems to divert w

EXCAVATION NOTES:

- . No blasting is anticipated for completion of the work shown. If owner is responsible for obtaining all necessary permits.
- There are no anticipated sales of excavated materials to the public
- 3. Bulk storage of fuel and lubricants for excavation equipment is n fueling and lubrication of equipment shall be completed on the fu shall be equipped with a spill kit and any spills shall be clea equipment service work which is likely to result in the release of take place on site.
- 4. The emergency contact for operations at this site is Paul Lehto (86
- The allowable hours of operation for excavation shall be 7:00 A through Friday and 7:00 AM to 12:00 noon on Saturday. No ope on Sundays, Christmas, New Years Day, Memorial Day, Fourth o Thanksaiving except by special permission of the Brooklyn Planning
- 6. The owner and/or site operator shall provide adequate dust control nuisance. The preferred dust control measure is the application travel areas. The application of calcium chloride may also be used
- 7. The owner/operator shall install any necessary barricades or barriers to provide protection around the perimeter of open excavation faces and steep slopes.
- 8. Excavation operations shall be completed in accordance with all appropriate Mine Safety & Health Administration (MSHA) rules and regulations.
- 9. There is to be no on-site processing of excavated materials.
- 10. The estimated total number of truck trip ends entering or exiting the site is 11,200 during the excavation duration. The estimated daily average number of truck trip ends entering or exiting the site is 60 during the excavation duration. The estimated maximum number of daily truck trip ends entering or exiting the site is 80.
- 11. The site operator is responsible for determining the most appropriate means and methods for excavating material. In general, excavation shall begin with stripping and stockpiling of topsoil and subsoil which will be utilized for site restoration. Topsoil (A horizon) and subsoil (B horizon) shall be stockpiled separately. Removal of material should be accomplished with a downcutting technique to ensure complete internal drainage at all times.
- 12. All trucks leaving the site shall have the loads covered.
- 13. Prior to the start of excavation work, two elevation bench marks shall be installed on the perimeter of the work area for monitoring purposes. Benchmarks shall be maintained or replaced as necessary as the work progresses.
- 14. It is anticipated that all excavation work will be completed with the use of one (1) wheel loader (Cat 980 or equivalent), one (1) 50 ton excavator (Cat 349 or equivalent). and triaxle dump trucks (16± CY capacity). Additional equipment may be utilized for final site restoration.

RESTORATION NOTES:

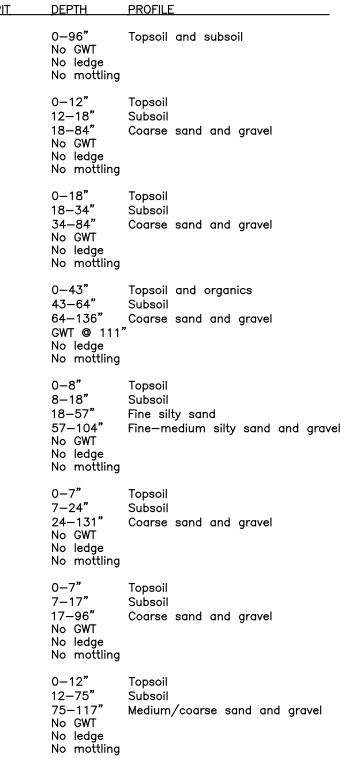
- The restoration requirements described below will be applicable to the 6.7 acre permitted area.
- Restoration of disturbed areas shall take place following the completion of excavation in the respective phase. The respective phase shall have subsoil and topsoil spread and be seeded and mulched no later than the end of the growing season for the calendar year following completion of excavation operations. Mulching and seeding shall be completed in accordance with the recommendations of the New York State Revegetation Procedures Manual for Surface Mining Reclamation. Sufficient restoration bonding should be maintained as required by the Town to cover the restoration cost for the permitted excavation area. The sediment/infiltration basin in the lowest part of the site shall not be restored with topsoil and vegetation until the completion of excavation in phase 2.
- Final restoration shall begin with establishing the required subgrade elevations. Proposed grades shown are approximate and may be adjusted to match field conditions at the time of restoration. In general, all disturbed slopes shall be graded to a 30% maximum

onstruction activities are ped on the premise of an to the inclusion of dule is a sequence with actions in a sequence a construction equipment ed vegetation. d at any one time are Clear only those areas are operational as soon before outletting storm completed as soon as num by absorbing and hereases as the volume runoff increases during he removal of existing of impervious surfaces. b break flow lines and ating the potential for	3. 4. 5. 6. 7. 8. 9.	Complete re minimum th stockpiles n topsoil as r Spread seed permanent of which is sui <u>Variety</u> Switchgras Big Blueste Little Bluest Sand Love Bird's-foot Hay or stro establishmer be allowable Fertilizer an based on lo Restoration minimum of In lieu of	estoration by s nickness of 6' nay be supple necessary to p d for a perm vegetative cov itable for use s (Blackwell, 3 em (Niagra, k stem (Blaze, A grass (NE-27 t Trefoil (Emp aw mulch sha t of permane s. d lime shall l boratory soil cover vegetat 24 months p the manual of	Aldous, Camper) , Bend) ire, Viking) III be utilized on ent vegetative cove testing results. tion shall be mai prior to the release application of mu
ating the potential for		planted with	hydroseeding	methods with a
should not be directed at the mixing of clean]	TEST PIT OBS	SERVATIONS -	AUGUST 7, 2017
ite until after adequate]	TEST PIT	DEPTH	PROFILE
and drainage ways that t runoff is trapped or	1	I	0–96" No GWT No ledge No mottling	Topsoil and subs
S point of discharge for	2	2	0-12" 12-18" 18-84" No GWT	Topsoil Subsoil Coarse sand and
ective to apply internal ducing sediment loading otential off—site damage ot off—site damage than ossible. It is easier to carried downstream and		3	No ledge No mottling 0–18" 18–34" 34–84" No GWT No ledge No mottling	Topsoil Subsoil Coarse sand and
getated areas to reduce ring of sediments. stable outlets using rip ures.	2	4	0–43" 43–64" 64–136" GWT @ 111 No ledge No mottling	Topsoil and orga Subsoil Coarse sand and "
are required on larger possible or impractical to ge and small sites when be impacted by off—site ands or permanent or prcept runoff prior to its	ţ	5	0-8" 8-18" 18-57" 57-104" No GWT No ledge No mottling	Topsoil Subsoil Fine silty sand Fine—medium silt
vater away from them.	e	5	0–7" 7–24" 24–131" No GWT No ledge No mottling	Topsoil Subsoil Coarse sand and
blasting is required, the from the subject site.	7	7	0-7" 7-17" 17-96"	Topsoil Subsoil Coarse sand and
ot allowed on site. All Jeling pad. Fuel trucks aned immediately. No fuel or lubricants shall	٤	3	No GWT No ledge No mottling 0–12"	Topsoil
60) 208—9789. M to 6:00 PM, Monday erations shall be allowed			12–75" 75–117" No GWT No ledge No mottling	Subsoil Medium/coarse s
to prevent any off-site of water to vehicular	ç	Э	0–10" 10–20" 20–138"	Topsoil Subsoil Coarse sand & o
I.				

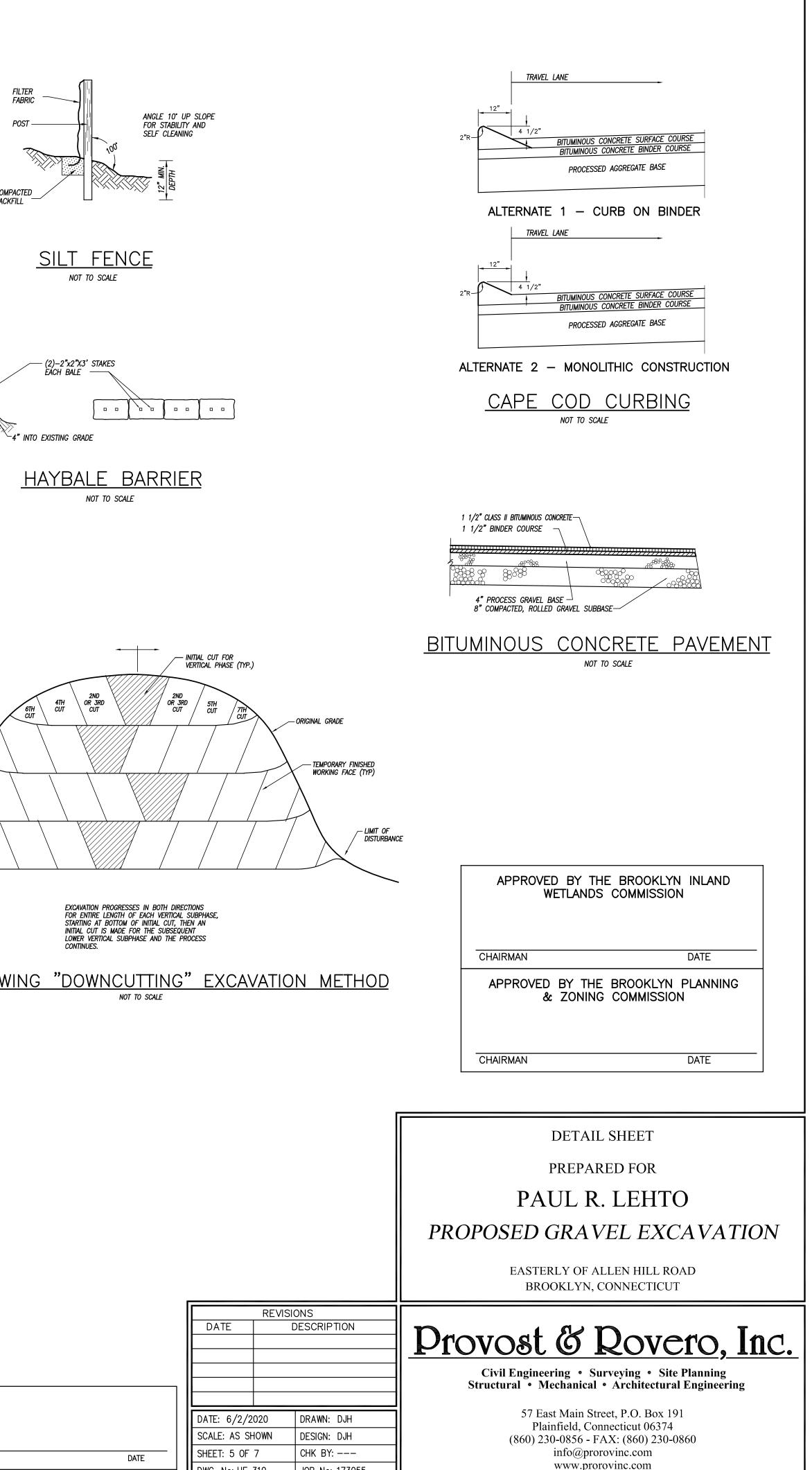
- spreading subsoil (B horizon) material to a uniform depth.
- ing on-site stockpiled topsoil (A horizon) to an approximate seeding for a permanent vegetative cover. On-site topsoil with composted organic matter, wood chips and imported a suitable planting medium.
- vegetative cover over the prepared restoration area. The be a suitable wildlife habitat mix or the following mixture locations:

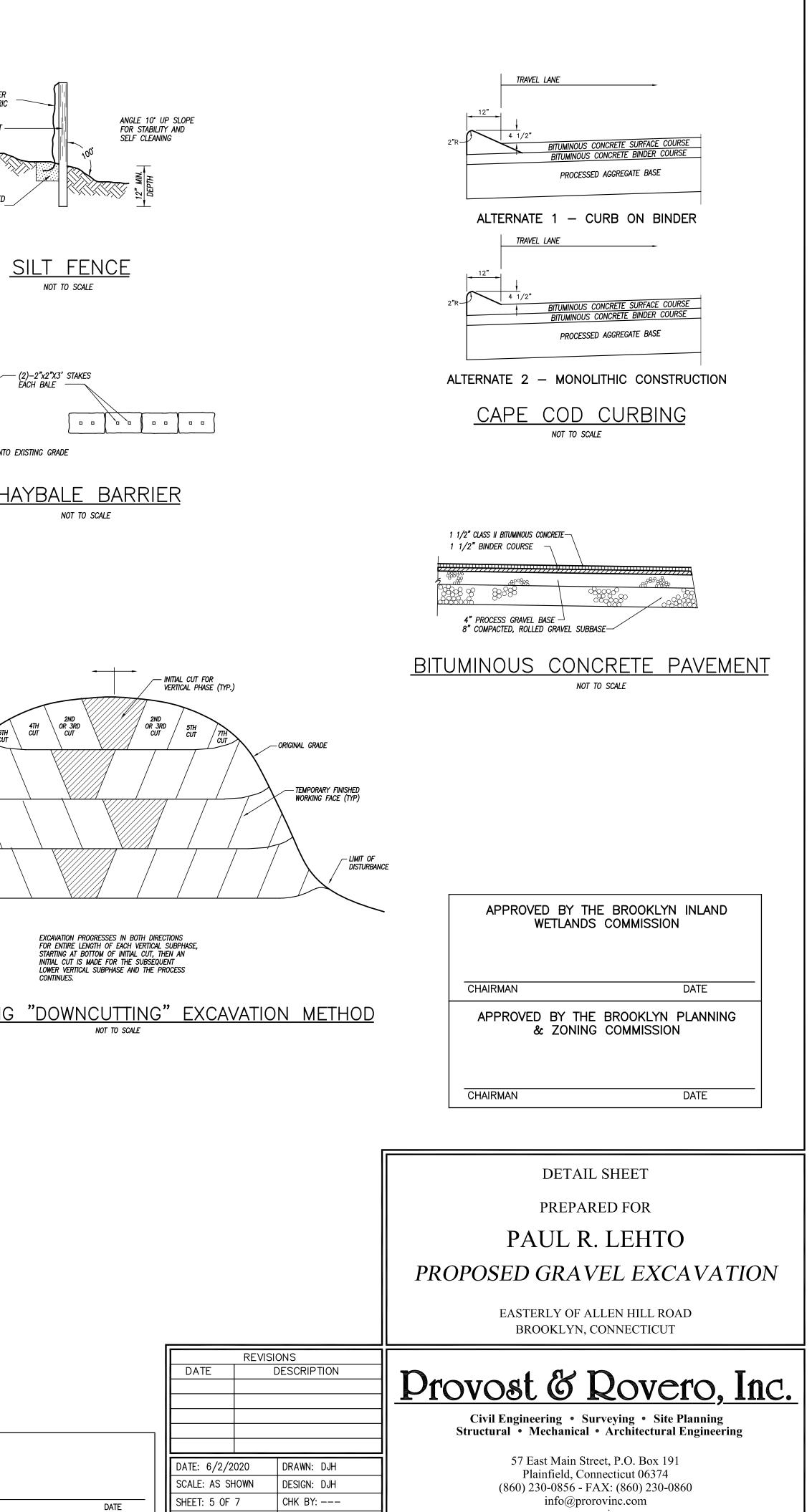
Variety	Lbs/Acre
Switchgrass (Blackwell, Shelter, Cave-in-rock)	4.0
Big Bluestem (Niagra, Kaw)	4.0
Little Bluestem (Blaze, Aldous, Camper)	2.0
Sand Lovegrass (NE-27, Bend)	1.5
Bird's-foot Trefoil (Empire, Viking)	2.0
· · · · · · · · · · · · · · · · · · ·	TOTAL 13.5

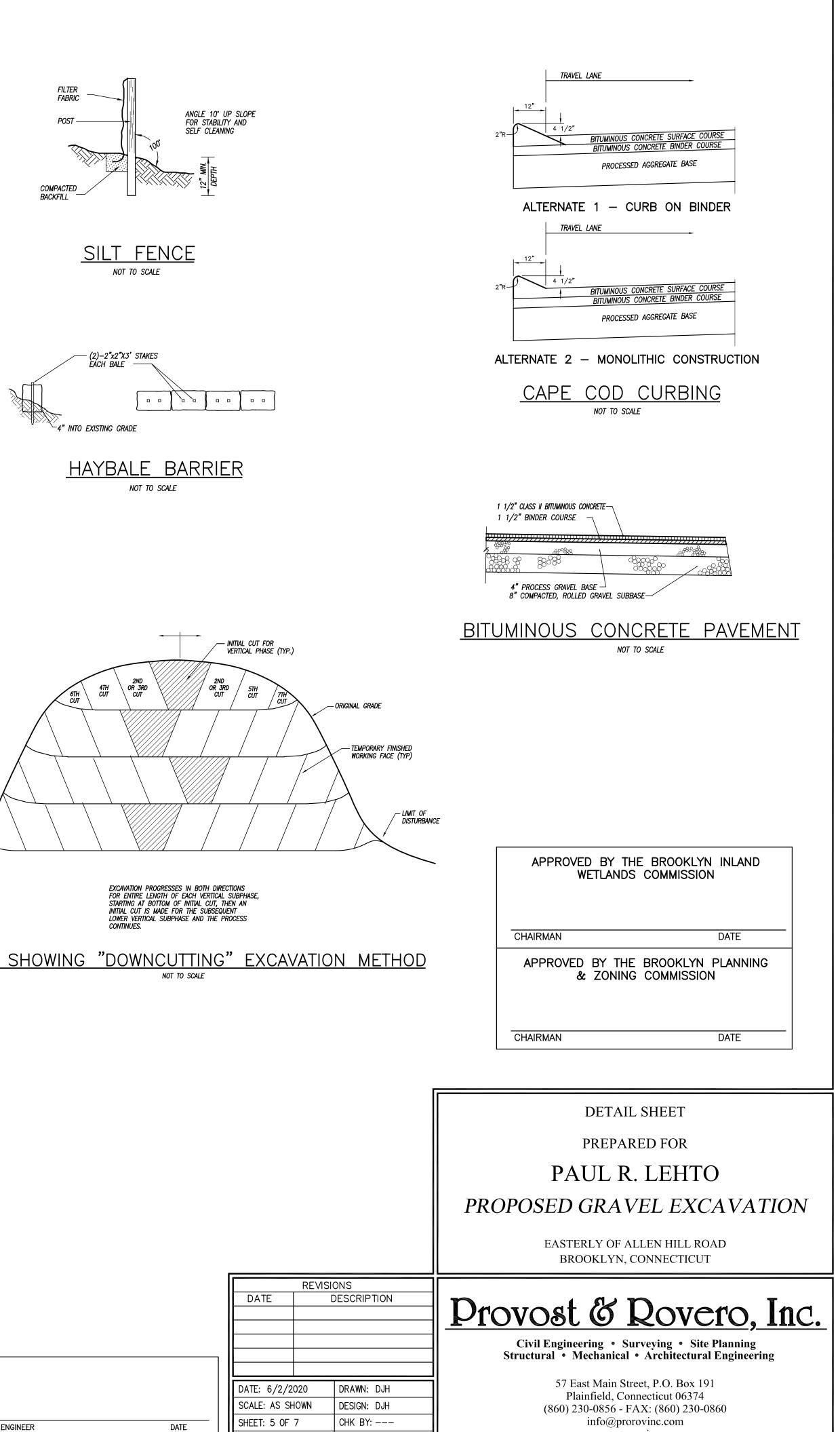
- utilized on slopes to provide temporary stabilization during etative cover. In general, no slopes greater than 2H:1V will
- vided as required to establish a permanent vegetative cover results all be maintained by the permit holder or applicant for a
- the release of any restoration bonding. tion of mulch and fertilizer, the restoration area may be ods with a suitable tackifier, mulch and fertilizer mix.

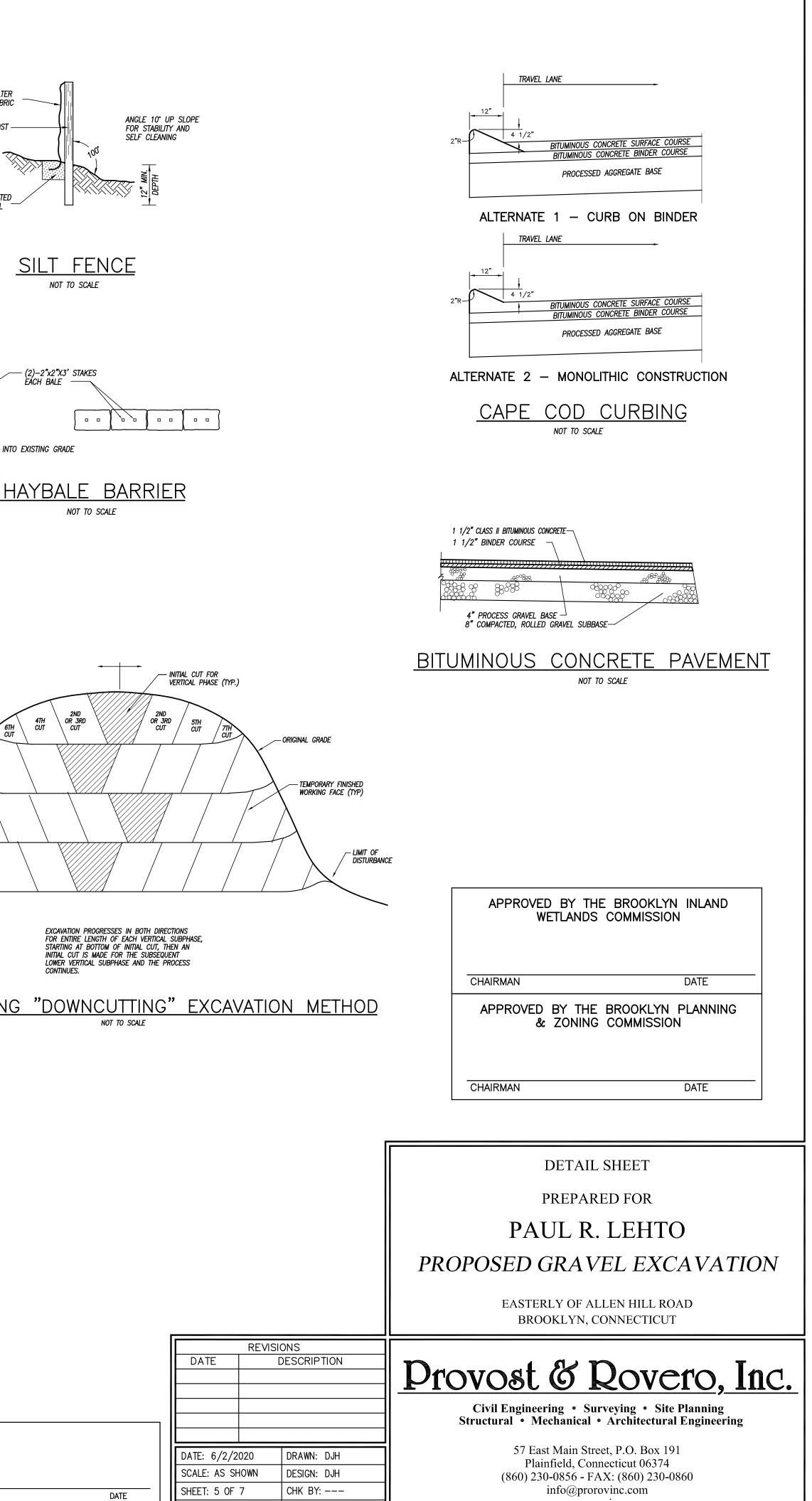


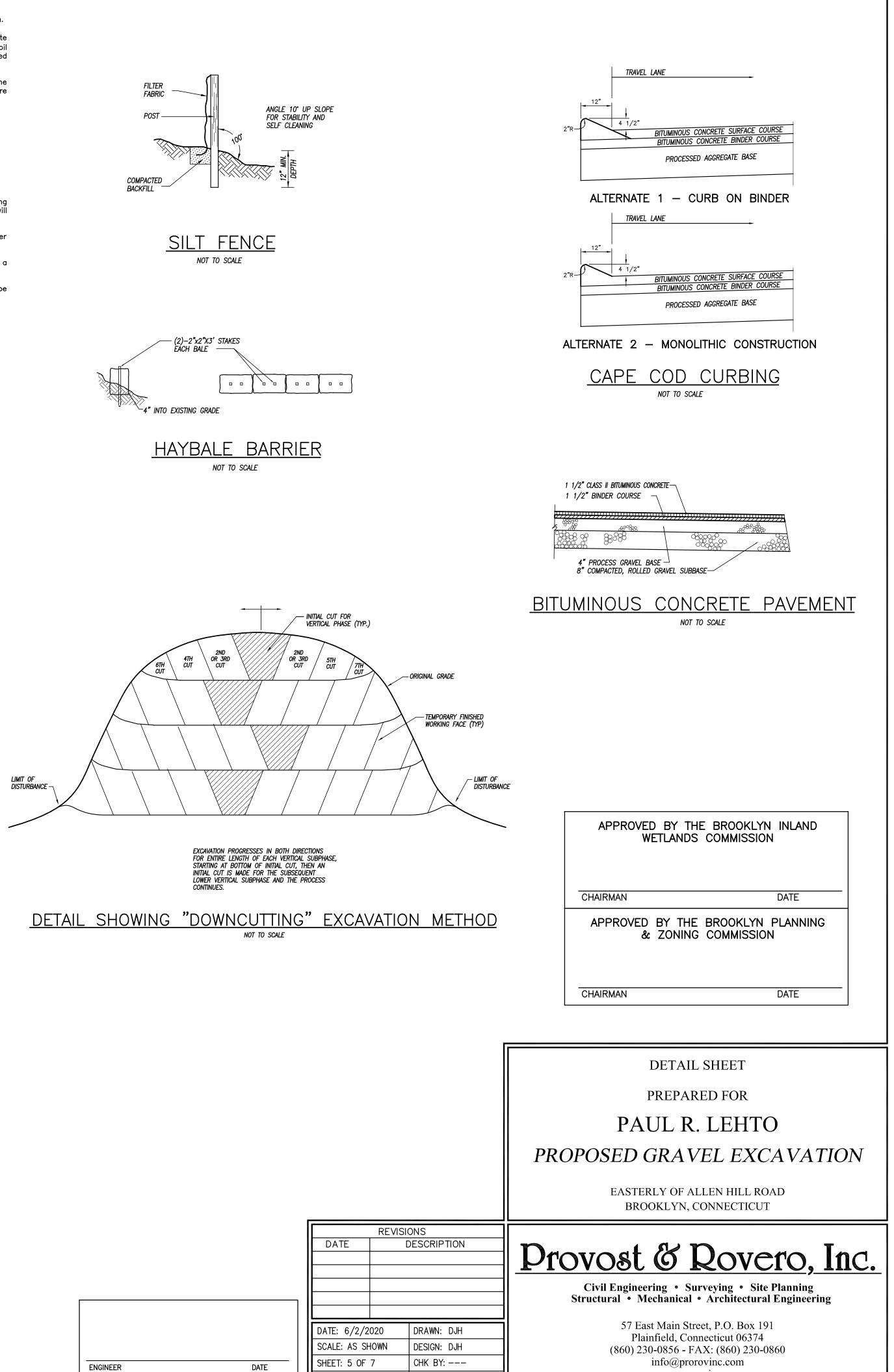
sand & gravel









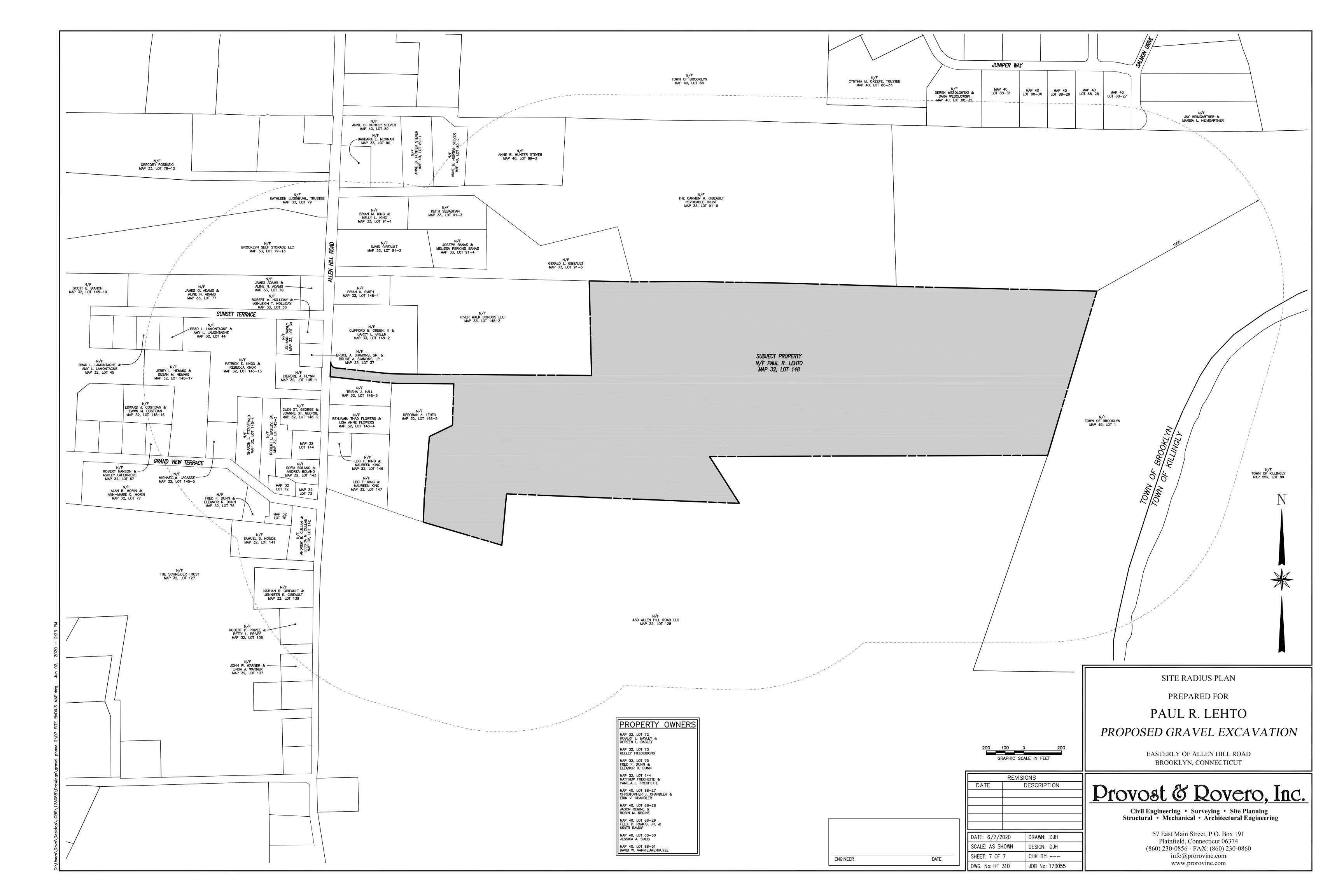


JOB No: 173055

DWG. No: HF 310



	N X X
OPEN SPACE J.J.BOT AC. (1,653,854 S.F.)	
	SITE REUSE PLAN CONCEPTUAL CONSERVATION SUBDIVISION PREPARED FOR
150 75 0 150 GRAPHIC SCALE IN FEET	PAUL R. LEHTO PROPOSED GRAVEL EXCAVATION EASTERLY OF ALLEN HILL ROAD
DATE DATE DESCRIPTION DATE DESCRIPTION DATE DATE: 6/2/2020 DRAWN: DJH SCALE: 1" = 150' DESIGN: DJH SHEET: 6 OF 7 CHK BY: DWG. No: HF 310 JOB No: 173055	BROOKLYN, CONNECTICUT Drovost & Dovero, Inc. Civil Engineering • Surveying • Site Planning Structural • Mechanical • Architectural Engineering 57 East Main Street, P.O. Box 191 Plainfield, Connecticut 06374 (860) 230-0856 - FAX: (860) 230-0860 info@prorovinc.com www.prorovinc.com



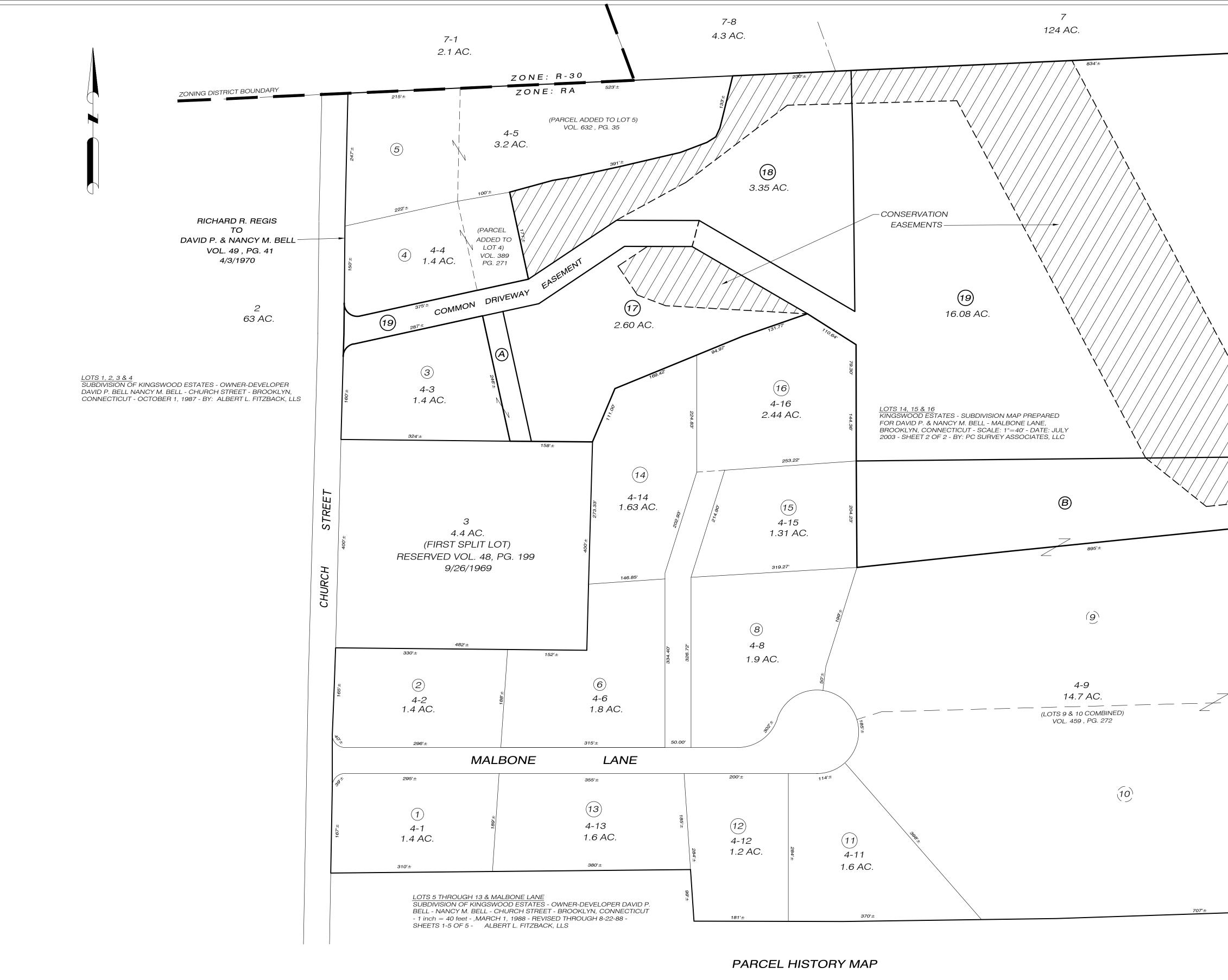
NORTHEASTERN CONNECTICUT COUNCIL OF GOVERNMENTS

Engineering Plan Review Pertaining to Proposed Gravel Excavation PAUL R. LEHTO (RIVER WALK DRIVE) BROOKLYN, CT (July 14, 2020)

The comments contained herein pertain to my review of plans for a gravel removal operation. The plans under review (7 sheets) are entitled "Proposed Gravel Excavation, Easterly of Allen Hill Road, Brooklyn, Connecticut, Owner/Applicant: Paul R. Lehto," prepared by Provost & Rovero, Inc. and Archer Surveying, LLC, dated June 2, 2020. This review was made in accordance with most recent Town of Brooklyn Zoning and Wetlands Regulations and Public Improvement Specifications.

- 1. On Sheet 2 of 7, "Existing Conditions," Note 6 states that the existing topographical information was created using aerial photography (and photogrammetric mapping?) from WSP Group. The dates for the photography and mapping should be included in the note.
- 2. If not already done, the CT State Historic Preservation Office should be contacted regarding any possible archaeological/historical significance to this portion of the site, since it sits high above and only about a quarter mile from the Quinebaug River. The CT Department of Energy and Environmental Protection (DEEP) "Natural Diversity Database" should also be consulted.
- 3. The haul road running through the previously excavated area to River Walk Drive (see Sheet 2 of 7) crosses a wetland. It is recommended that the Applicant's engineer evaluate and describe the crossing, which has been in place for many years, to determine if it is in good condition for future heavy loads and if any erosion has occurred around it that would require some reconstruction. Additionally, it is important to establish erosion and sediment control systems on both sides of the crossing and other methods to help protect the wetlands from the heavy truck traffic, dust, and material that may fly off haul trucks. Erosion and sediment control system(s), if required, should be shown for the affected area on a plan at a scale of no less than 1" = 40'.
- 4. There is no estimated time of completion of the proposed gravel removal operation in the "Excavation Notes" on Sheet 5 of 7.
- 5. Noise and dust from heavy truck traffic may cause an issue with residents living along River Walk Drive and its connected side roads

7/14/2020 By: Syl Pauley, Jr., P.E., NECCOG Regional E Lehto Gravel Operation Plan Review Comments 07_14_2020.doc



APPROVED BY THE BROOKLYN PLANNING & ZONING COMMISSION

CHAIRMAN

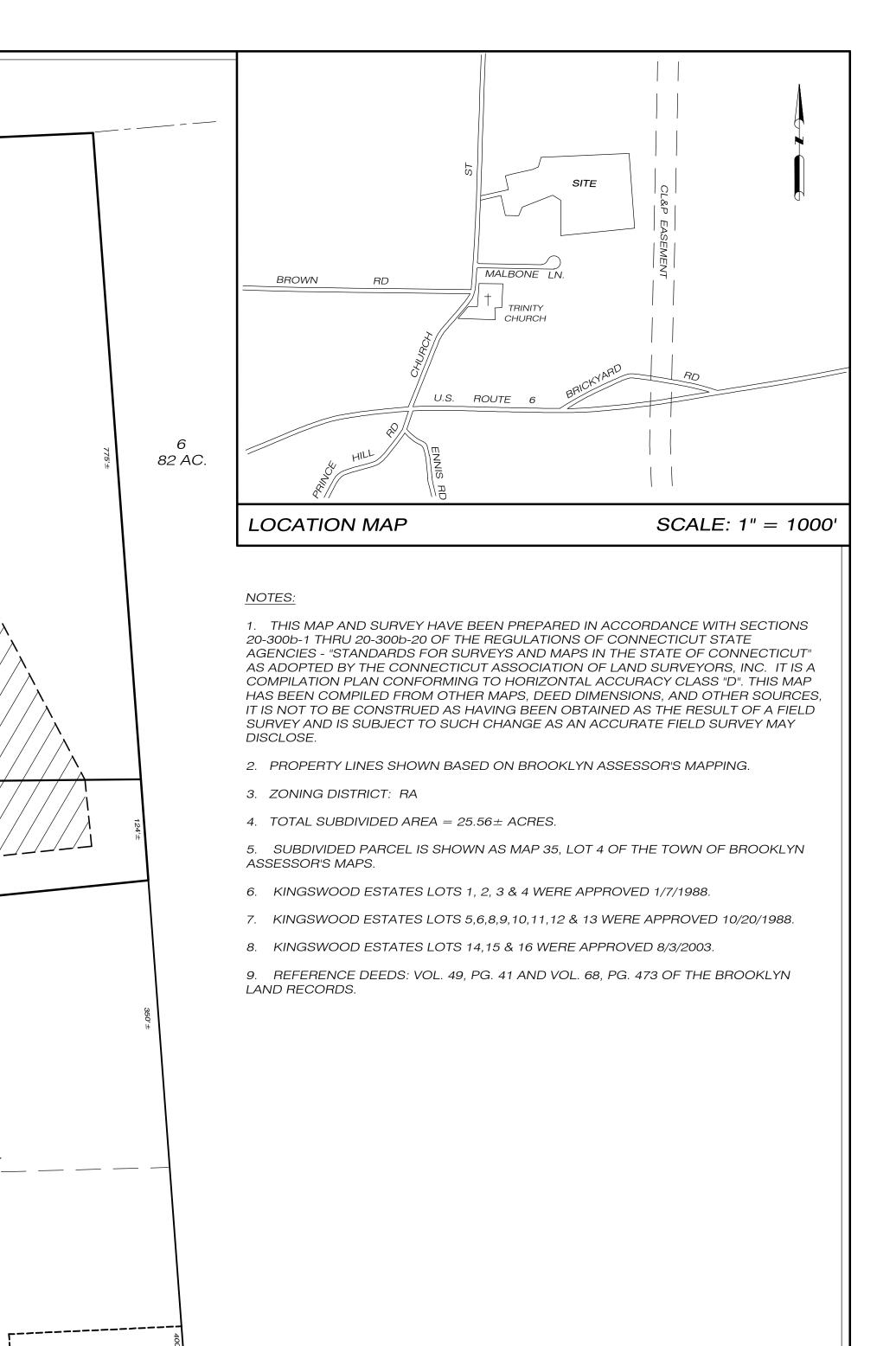
DATE

PER SECTION 8-26 OF THE CONNECTICUT GENERAL STATUTES, AMENDED, APPROVAL AUTOMATICALLY EXPIRES IF ALL PHYSICAL IMPROVEMENTS REQUIRED BY THIS PLAN ARE NOT COMPLETED BY THAT DATE



TO THE BEST OF MY KNOWLEDGE AND BELIEF, THIS MAP IS SUBSTANTIALLY

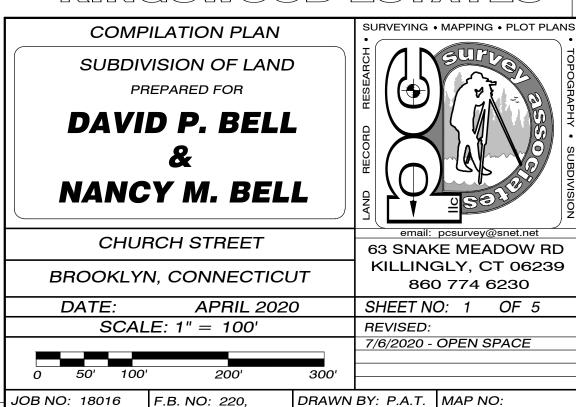
PAULA. TERWILLIGER, L.L.S. NO. 10155 NO CERTIFICATION IS EXPRESSED OR IMPLIED UNLESS THIS MAP BEARS THE EMBOSSED SEAL OF THE LAND SURVEYOR WHOSE SIGNATURE APPEARS HEREON.



RICHARD R. REGIS то - DAVID P. & NANCY M. BELL VOL. 68 , PG. 473 4/3/1970

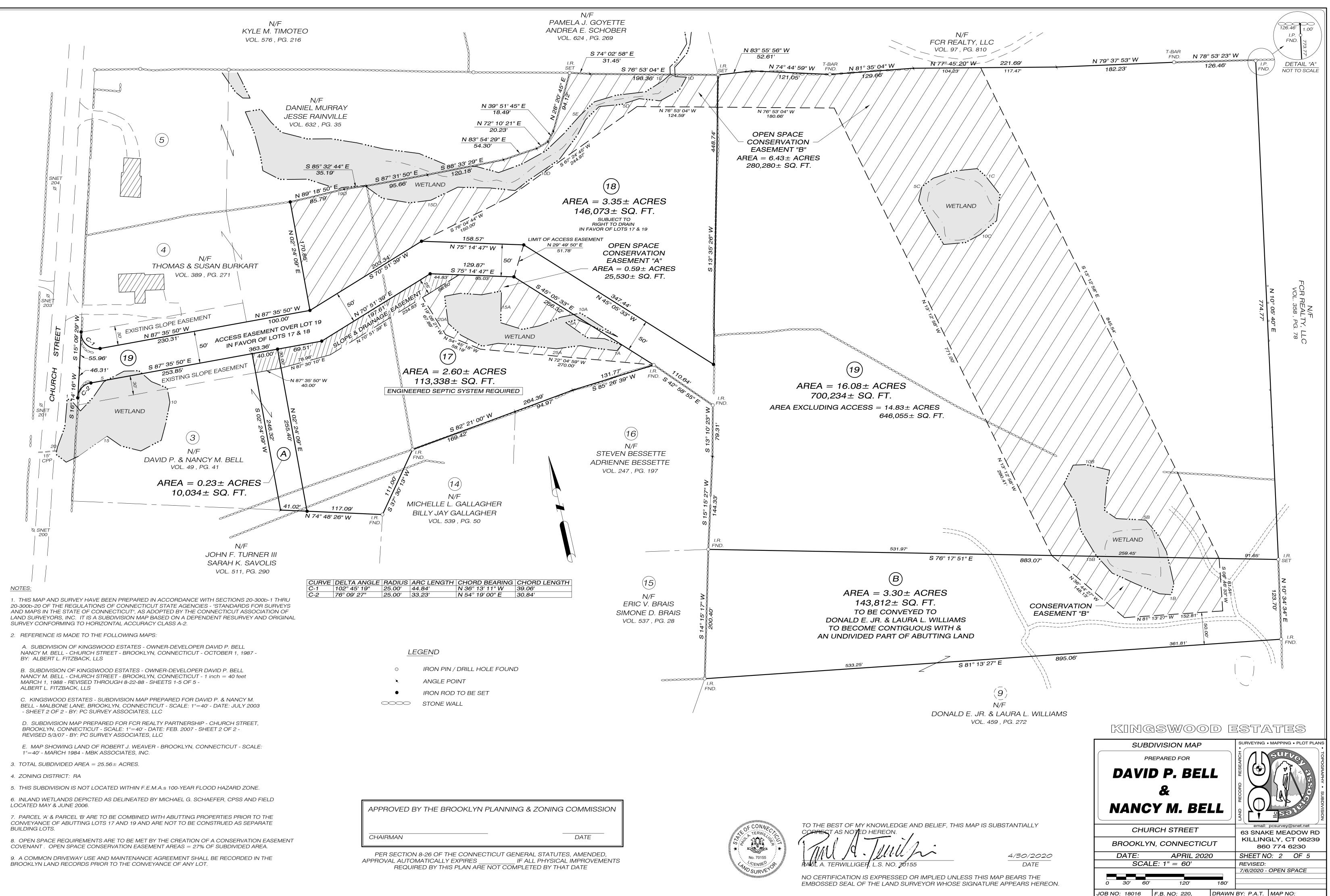
> OWNER/APPLICANT: DAVID P. & NANCY M. BELL PO BOX 358 BROOKLYN, CT 06234

KINGSWOOD ESTATES



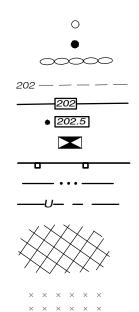
7/6/2020 DATE

-





LEGEND



 $\times \times \times \times \times \times$

 $\times \times \times \times \times \times$

IRON PIN FOUND IRON ROD TO BE SET STONE WALL EXISTING CONTOUR PROPOSED CONTOUR PROPOSED SPOT GRADE TEST PIT EROSION CONTROL BARRIER EDGE OF WETLAND PROPOSED UNDERGROUND UTILITIES

SLOPES GREATER THAN 15%

FARMLAND SOILS

PROPOSED CLEARING LIMIT



APPROVED BY THE BROOKLYN INLAND WETLANDS & WATERCOURSES COMMISSION

CHAIRMAN

APPROVED BY THE BROOKLYN PLANNING & ZONING COMMISSION

CHAIRMAN

PER SECTION 8-26 OF THE CONNECTICUT GENERAL STATUTES, AMENDED, APPROVAL AUTOMATICALLY EXPIRES ______ IF ALL PHYSICAL IMPROVEMENTS REQUIRED BY THIS PLAN ARE NOT COMPLETED BY THAT DATE

No. 70155

S NOTED**/**HEREON TERWILLIGER, L. . NO. 70155

4/30/2020 DATE

DATE

DATE

NO CERTIFICATION IS EXPRESSED OR IMPLIED UNLESS THIS MAP BEARS THE EMBOSSED SEAL OF THE LAND SURVEYOR WHOSE SIGNATURE APPEARS HEREON

TO THE BEST OF MY KNOWLEDGE AND BELIEF, THIS MAP IS SUBSTANTIALLY

NOTES:

1. THIS MAP AND SURVEY HAVE BEEN PREPARED IN ACCORDANCE WITH SECTIONS 20-300b-1 THRU 20-300b-20 OF THE REGULATIONS OF CONNECTICUT STATE AGENCIES - "STANDARDS FOR SURVEYS AND MAPS IN THE STATE OF CONNECTICUT", AS ADOPTED BY THE CONNECTICUT ASSOCIATION OF LAND SURVEYORS, INC. IT IS A COMPILATION MAP BASED ON A DEPENDENT RESURVEY CONFORMING TO HORIZONTAL ACCURACY CLASS "D". SEE SHEETS 2 & 4 FOR PROPERTY LINE & LOT DEVELOPMENT INFORMATION. TOPOGRAPHIC FEATURES DEPICTED HEREON WERE TAKEN FROM AERIAL PHOTOGRAMMETRY PROVIDED BY CHAS. H. SELLS, INC. DATED JUNE 2006 AND CONFORM TO TOPOGRAPHIC ACCURACY CLASS T-3. VERTICAL DATUM IS NGVD88. THIS MAP HAS BEEN COMPILED FROM OTHER MAPS, DEED DIMENSIONS, AND OTHER SOURCES, IT IS NOT TO BE CONSTRUED AS HAVING BEEN OBTAINED AS THE RESULT OF A FIELD SURVEY AND IS SUBJECT TO SUCH CHANGE AS AN ACCURATE FIELD SURVEY MAY DISCLOSE.

2. ZONING DISTRICT: RA

3. PROPOSED IMPROVEMENTS ARE CONCEPTUAL LOCATIONS TO SHOW LOT SUITABILITY ONLY.

4. SOLAR ACCESS WAS CONSIDERED IN THE DESIGN OF THIS SUBDIVISION. THE HOUSE LOCATIONS DEPICTED ARE ONLY CONCEPTUAL IN NATURE AND IT IS UP TO THE LOT DEVELOPER TO TAKE ADVANTAGE OF THE PASSIVE SOLAR OPPORTUNITIES PRESENTED BY THESE LOTS AT THE TIME OF ACTUAL HOUSE CONSTRUCTION. THE DEVELOPER IS ENCOURAGED TO UTILIZE PASSIVE SOLAR TECHNIQUES AND IT IS RECOMMENDED THAT SUCH FACTORS AS HOUSE ORIENTATION, WINDOW LOCATION AND STYLE, CLEARING LIMITS AND POSITION ON THE LOT BE TAKEN INTO CONSIDERATION WHEN DEVELOPMENT OCCURS.

5. THE INLAND WETLANDS & WATERCOURSES WERE FIELD DELINEATED BY MICHAEL G. SCHAEFER, SOIL SCIENTIST AND FIELD LOCATED BY PC SURVEY ASSOCIATES, LLC IN MAY AND JUNE 2006.

6. MAXIMUM DRIVEWAY GRADE PERMITTED IS 12%. GRADES OF 10% OR GREATER ARE TO BE PAVED. PROPOSED DRIVEWAY GRADES DEPICTED ARE AT LESS THAN 10%.

SOILS WITHIN DEVELOPOMENT AREAS

CHARTLTON-CHATFIELD COMPLEX, 0-15% SLOPES, VERY ROCKY FINE SANDY LOAM TO GRAVELLY FINE SANDY LOAM WELL DRAINED, WATER TABLE GREATER THAN 80", BEDROCK 20" - 80"

SUTTON, 0-8% SLOPES, VERY STONY FINE SANDY LOAM TO GRAVELLY SANDY LOAM MODERATELY WELL DRAINED, WATER TABLE 12-27"

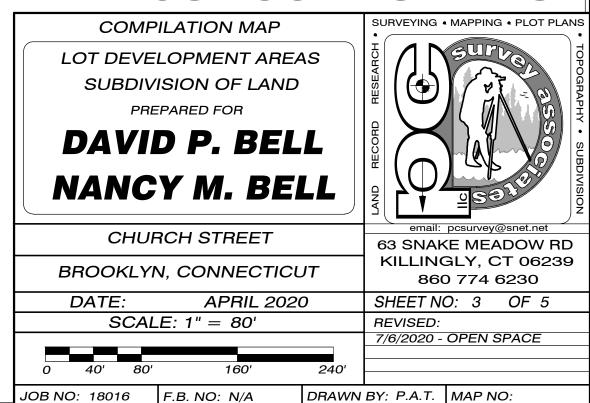
FARMLAND SOILS ON SUBDIVIDED PROPERTY

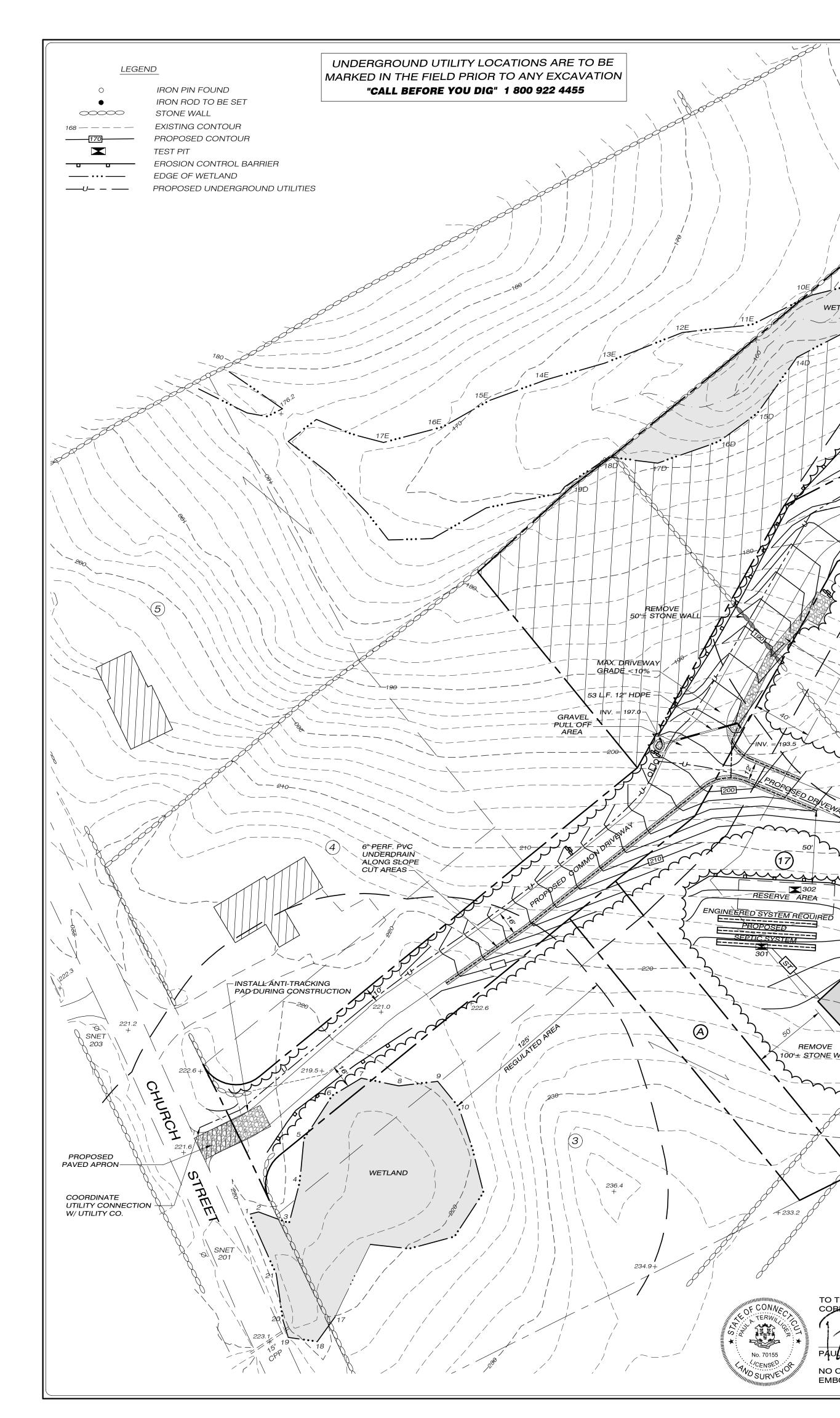
NINIGRET FINE SANDY LOAM, 0-3% SLOPES CANTON & CHARLTON FINE SANDY LOAMS, 3-8% SLOPES HINKLEY LOAMY SAND, 3-15% SLOPES WALPOLE LOAMY SAND, 0-3% SLOPES AREA OF FARMLAND SOILS: 10± ACRES

WETLAND SOILS ON SUBDIVIDED PROPERTY RIDGEBURY & LEICESTER WALPOLE

* SOILS INFORMATION AS TAKEN FROM USDA NRCS WEBSITE

KINGSWOOD ESTATES







ULATED AR

ROPOSE

WETLAND

- MOD. RIP-RAP DRAINAGE SWALE-

REMOVE 00' STONE WALL LIMIT OF -

COMMON DRIVEWAY

NOTES

1. THIS MAP AND SURVEY HAVE BEEN PREPARED IN ACCORDANCE WITH SECTIONS 20-300b-1 THRU 20-300b-20 OF THE REGULATIONS OF CONNECTICUT STATE AGENCIES - "STANDARDS FOR SURVEYS AND MAPS IN THE STATE OF CONNECTICUT", AS ADOPTED BY THE CONNECTICUT ASSOCIATION OF LAND SURVEYORS, INC. IT IS A GENERAL LOCATION SURVEY BASED ON A DEPENDENT RESURVEY AND ORIGINAL SURVEY CONFORMING TO HORIZONTAL ACCURACY CLASS "C". SEE SHEET 2 FOR PROPERTY LINE INFORMATION. TOPOGRAPHIC FEATURES DEPICTED HEREON WERE TAKEN FROM AERIAL PHOTOGRAMMETRY PROVIDED BY CHAS. H. SELLS, INC. DATED JUNE 2006 AND CONFORM TO TOPOGRAPHIC ACCURACY CLASS T-3. VERTICAL DATUM IS NGVD88. CONTOUR INTERVAL: 2 FEET.

2. ZONING DISTRICT: RA

3. PROPOSED IMPROVEMENTS ARE CONCEPTUAL LOCATIONS TO SHOW LOT SUITABILITY ONLY.

4. SOLAR ACCESS WAS CONSIDERED IN THE DESIGN OF THIS SUBDIVISION. THE HOUSE LOCATIONS DEPICTED ARE ONLY CONCEPTUAL IN NATURE AND IT IS UP TO THE LOT DEVELOPER TO TAKE ADVANTAGE OF THE PASSIVE SOLAR OPPORTUNITIES PRESENTED BY THESE LOTS AT THE TIME OF ACTUAL HOUSE CONSTRUCTION. THE DEVELOPER IS ENCOURAGED TO UTILIZE PASSIVE SOLAR TECHNIQUES AND IT IS RECOMMENDED THAT SUCH FACTORS AS HOUSE ORIENTATION, WINDOW LOCATION AND STYLE, CLEARING LIMITS AND POSITION ON THE LOT BE TAKEN INTO CONSIDERATION WHEN DEVELOPMENT OCCURS.

5. THE INLAND WETLANDS & WATERCOURSES WERE FIELD DELINEATED BY MICHAEL G. SCHAEFER, SOIL SCIENTIST AND FIELD LOCATED BY PC SURVEY ASSOCIATES, LLC IN MAY AND JUNE 2006.

6. MAXIMUM DRIVEWAY GRADE PERMITTED IS 12%. GRADES OF 10% OR GREATER ARE TO BE PAVED. PROPOSED DRIVEWAY GRADES DEPICTED ARE AT LESS THAN 10%.

7. SEE SHEET 5 FOR EROSION & SEDIMENT CONTROL PLAN.

APPROVED BY THE BROOKLYN INLAND WETLANDS & WATERCOURSES COMMISSION

CHAIRMAN

APPROVED BY THE BROOKLYN PLANNING & ZONING COMMISSION

CHAIRMAN

PER SECTION 8-26 OF THE CONNECTICUT GENERAL STATUTES, AMENDED, APPROVAL AUTOMATICALLY EXPIRES REQUIRED BY THIS PLAN ARE NOT COMPLETED BY THAT DATE

TO THE BEST OF MY KNOWLEDGE AND BELIEF, THIS MAP IS SUBSTANTIALLY HEBEO

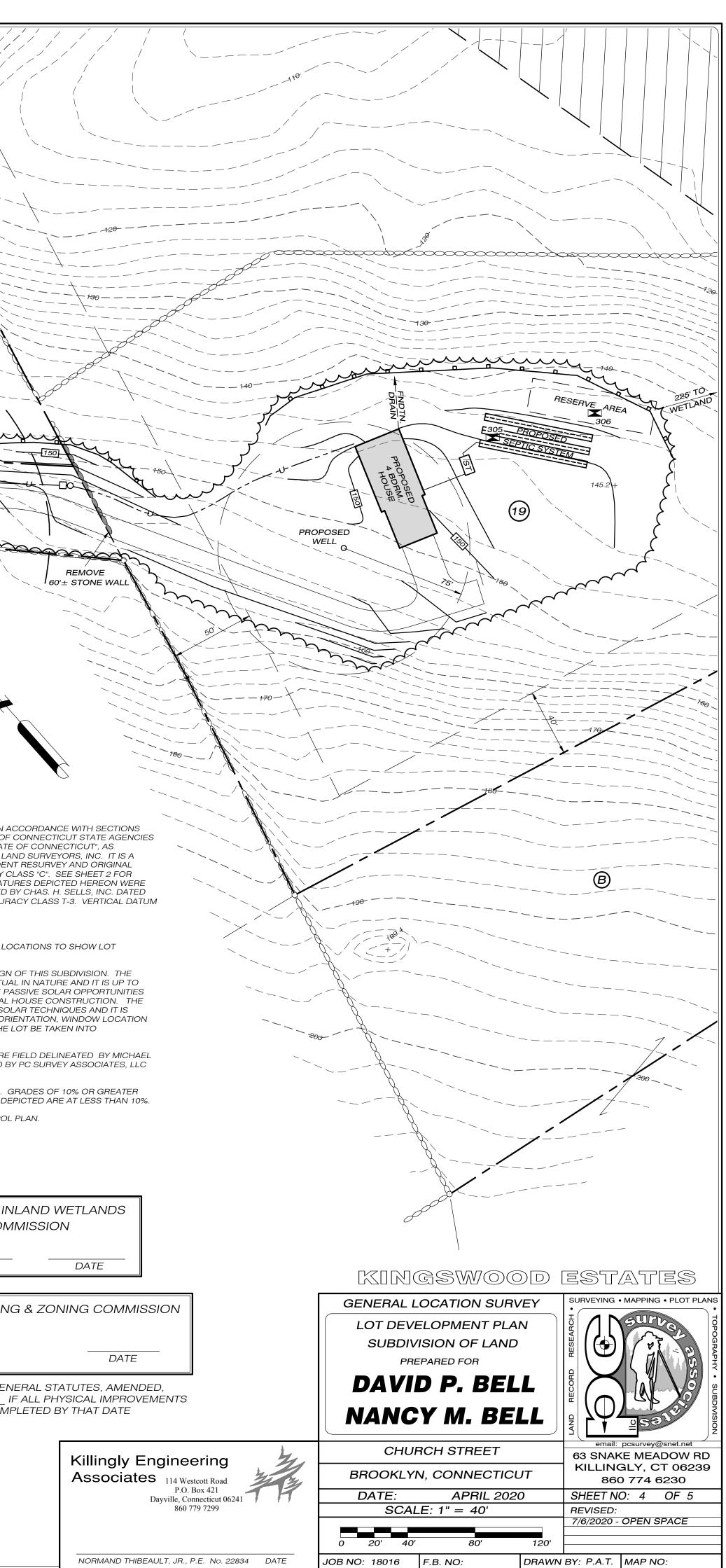
TERWIĽLIGER, NO. 7015

PROPOSEL

4/30/2020 DATE

NO CERTIFICATION IS EXPRESSED OR IMPLIED UNLESS THIS MAP BEARS THE EMBOSSED SEAL OF THE LAND SURVEYOR WHOSE SIGNATURE APPEARS HEREON

_ _ _



ALL EROSION AND SEDIMENT CONTROL MEASURES AND PROCEDURES SHALL CONFORM TO CONNECTICUT GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL, MAY 2002.

DEVELOPMENT

PROPOSED DEVELOPMENT WILL CREATE 3 NEW BUILDING LOTS. ACTIVITIES TO INCLUDE CONSTRUCTION OF A COMMON AND INDIVIDUAL DRIVEWAYS, HOUSES, SEPTIC SYSTEMS, WELLS, AND SITE GRADING. THE PRIMARY CONCERN OF THIS EROSION & SEDIMENT CONTROL PLAN IS TO PREVENT EXCESSIVE EROSION AND KEEP ERODED SEDIMENT FROM RUNNING OFF SITE OF THE PROPOSED DEVELOPMENT OR INTO WETLAND AREAS. NO MATERIAL SHALL BE PLACED WITHIN A REGULATED WETLAND AREA EITHER ON OR OFF SITE.

CONSTRUCTION SEQUENCE: (INDIVIDUAL LOT DEVELOPMENT)

- 1. INSTALL EROSION AND SEDIMENT CONTROL MEASURES ALONG DOWN SLOPE SIDE OF THE PROPOSED LIMITS OF DISTURBANCE.
- 2. STRIP & STOCKPILE TOPSOIL.
- 3. PROVIDE ANTI TRACKING PAD AND TEMPORARY POWER TO THE SITE.
- 4. EXCAVATE FOUNDATION AND BEGIN CONSTRUCTION OF RESIDENCE.
- 5. INSTALL SEPTIC SYSTEM AND WELL
- 6. PROVIDE DRIVEWAY AND UTILITIES TO THE RESIDENCE.
- 7. LOAM. SEED & MULCH DISTURBED AREAS

8. REMOVE EROSION AND SEDIMENT CONTROL WHEN VEGETATIVE COVER HAS BEEN ESTABLISHED

GENERAL DEVELOPMENT PLAN

PRIOR TO THE COMMENCEMENT OF OPERATIONS IN ACCORDANCE WITH ANY PERMIT ISSUED BY THE TOWN OF BROOKLYN PLANNING AND ZONING COMMISSION, THE CONTRACTOR SHALL INSTALL ALL EROSION AND SEDIMENT CONTROL DEVICES.

THE CONTRACTOR SHALL OBTAIN A SITE INSPECTION FROM THE TOWN OF BROOKLYN ZONING ENFORCEMENT OFFICER OR WETLANDS AGENT TO ENSURE THAT ALL EROSION AND SEDIMENT CONTROL MEASURES HAVE BEEN INSTALLED IN ACCORDANCE WITH THIS NARRATIVE. UPON APPROVAL WITH RESPECT TO THE INSTALLATION OF EROSION AND SEDIMENT CONTROL MEASURES, THE CONTRACTOR MAY COMMENCE OPERATIONS PURSUANT TO THE PERMIT. EROSION AND SEDIMENT CONTROL DEVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE "SILT FENCE INSTALLATION & MAINTENANCE" AND "HAY BALE INSTALLATION & MAINTENANCE" SECTIONS OF THIS NARRATIVE.

ALL STRIPPING IS TO BE CONFINED TO THE IMMEDIATE CONSTRUCTION AREA. TOPSOIL SHALL BE STOCKPILED SO THAT SLOPES DO NOT EXCEED 2 TO 1. THERE SHALL BE NO BURIAL OF STUMPS. A HAY BALE OR SILT FENCE SEDIMENT BARRIER IS TO SURROUND EACH STOCKPILE AND A TEMPORARY VEGETATIVE COVER PROVIDED IF NECESSARY.

DUST CONTROL WILL BE ACCOMPLISHED BY SPRAYING WITH WATER.

FINAL STABILIZATION OF THE SITE IS TO FOLLOW THE PROCEDURES OUTLINED IN PERMANENT VEGETATIVE COVER. IF NECESSARY A TEMPORARY VEGETATIVE COVER IS TO BE PROVIDED UNTIL A PERMANENT COVER CAN BE APPLIED.

DURING THE STABILIZATION PERIOD, ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED IN PROPER WORKING ORDER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CHECKING ALL EROSION AND SEDIMENT CONTROL ON A TWICE-WEEKLY BASIS DURING THE STABILIZATION PERIOD AND AFTER EACH STORM EVENT. DURING THE STABILIZATION PERIOD WITH RESPECT TO EACH SITE, ANY EROSION WHICH OCCURS WITHIN DISTURBED AREAS SHALL BE IMMEDIATELY REPAIRED, RESEEDED AND RE-ESTABLISHED.

ALL DISTURBED SLOPES SHALL BE STABILIZED WITHIN ONE SEASON (SPRING OR FALL) OF THE COMPLETION OF THE PROJECT BEFORE A CERTIFICATE OF COMPLIANCE WILL BE ISSUED

ONCE STABILIZATION HAS BEEN COMPLETED AND APPROVED BY THE TOWN OF BROOKLYN ZONING ENFORCEMENT OFFICER, EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED BY THE CONTRACTOR.

SILT FENCE INSTALLATION AND MAINTENANCE.

1. DIG A 6" DEEP TRENCH ON THE UPHILL SIDE OF THE BARRIER LOCATION.

2. POSITION THE POSTS ON THE DOWNHILL SIDE OF THE BARRIER AND DRIVE THE POSTS 1.5 FEET INTO THE GROUND.

3. LAY THE BOTTOM 6" OF THE FABRIC IN THE TRENCH TO PREVENT UNDERMINING AND BACKFILL.

4. INSPECT AND REPAIR BARRIER AFTER HEAVY RAINFALL

5. INSPECTIONS WILL BE MADE AT LEAST ONCE PER WEEK AND WITHIN 24 HOURS OF THE END OF A STORM WITH A RAINFALL AMOUNT OF 0.5 INCH OR GREATER TO DETERMINE MAINTENANCE NEEDS.

6. SEDIMENT DEPOSITS ARE TO BE REMOVED WHEN THEY REACH A HEIGHT OF 1 FOOT BEHIND THE BARRIER OR HALF THE HEIGHT OF THE BARRIER AND ARE TO BE DEPOSITED IN AN AREA WHICH IS NOT REGULATED BY THE INLAND WETLANDS COMMISSION.

7. REPLACE OR REPAIR THE FENCE WITHIN 24 HOURS OF OBSERVED FAILURE. FAILURE OF THE FENCE HAS OCCURRED WHEN SEDIMENT FAILS TO BE RETAINED BY THE FENCE BECAUSE:

- THE FENCE HAS BEEN OVERTOPPED, UNDERCUT OR BYPASSED BY RUNOFF WATER, - THE FENCE HAS BEEN MOVED OUT OF POSITION, OR

- THE GEOTEXTILE HAS DECOMPOSED OR BEEN DAMAGED

HAY BALE INSTALLATION AND MAINTENANCE:

1. BALES SHALL BE PLACED AS SHOWN ON THE PLANS WITH THE ENDS OF THE BALES TIGHTLY ABUTTING EACH OTHER.

2. EACH BALE SHALL BE SECURELY ANCHORED WITH AT LEAST 2 STAKES AND GAPS BETWEEN BALES SHALL BE WEDGED WITH STRAW TO PREVENT WATER FROM PASSING BETWEEN THE BALES.

3. INSPECT BALES AT LEAST ONCE PER WEEK AND WITHIN 24 HOURS OF THE END OF A STORM WITH A RAINFALL AMOUNT OF 0.5 INCHES OR GREATER TO DETERMINE MAINTENANCE NEEDS.

4. REMOVE SEDIMENT BEHIND THE BALES WHEN IT REACHES HALF THE HEIGHT OF THE BALE AND DEPOSIT IN AN AREA WHICH IS NOT REGULATED BY THE INLAND WETLANDS COMMISSION.

5. REPLACE OR REPAIR THE BARRIER WITHIN 24 HOURS OF OBSERVED FAILURE. FAILURE OF THE BARRIER HAS OCCURRED WHEN SEDIMENT FAILS TO BE RETAINED BY THE BARRIER BECAUSE:

- THE BARRIER HAS BEEN OVERTOPPED, UNDERCUT OR BYPASSED BY RUNOFF WATER.

- THE BARRIER HAS BEEN MOVED OUT OF POSITION, OR - THE HAY BALES HAVE DETERIORATED OR BEEN DAMAGED

TEMPORARY VEGETATIVE COVER

OF STOCKPILES. IF THE SOIL HAS BEEN COMPACTED BY CONSTRUCTION

PERMANENT VEGETATIVE COVER

FOLLOWING GRASS SEED MIX:

SEED MIXTURE KENTUCKY BLUEGRASS CREEPING RED FESCUE PERENNIAL RYEGRASS

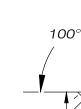
OF 3:1 OR GREATER SLOPE

BACKFILL & COMPACT EXCAVATED FILL ALONG HAY BALE

WEDGE LOOSE HAY BETWEEN BALES

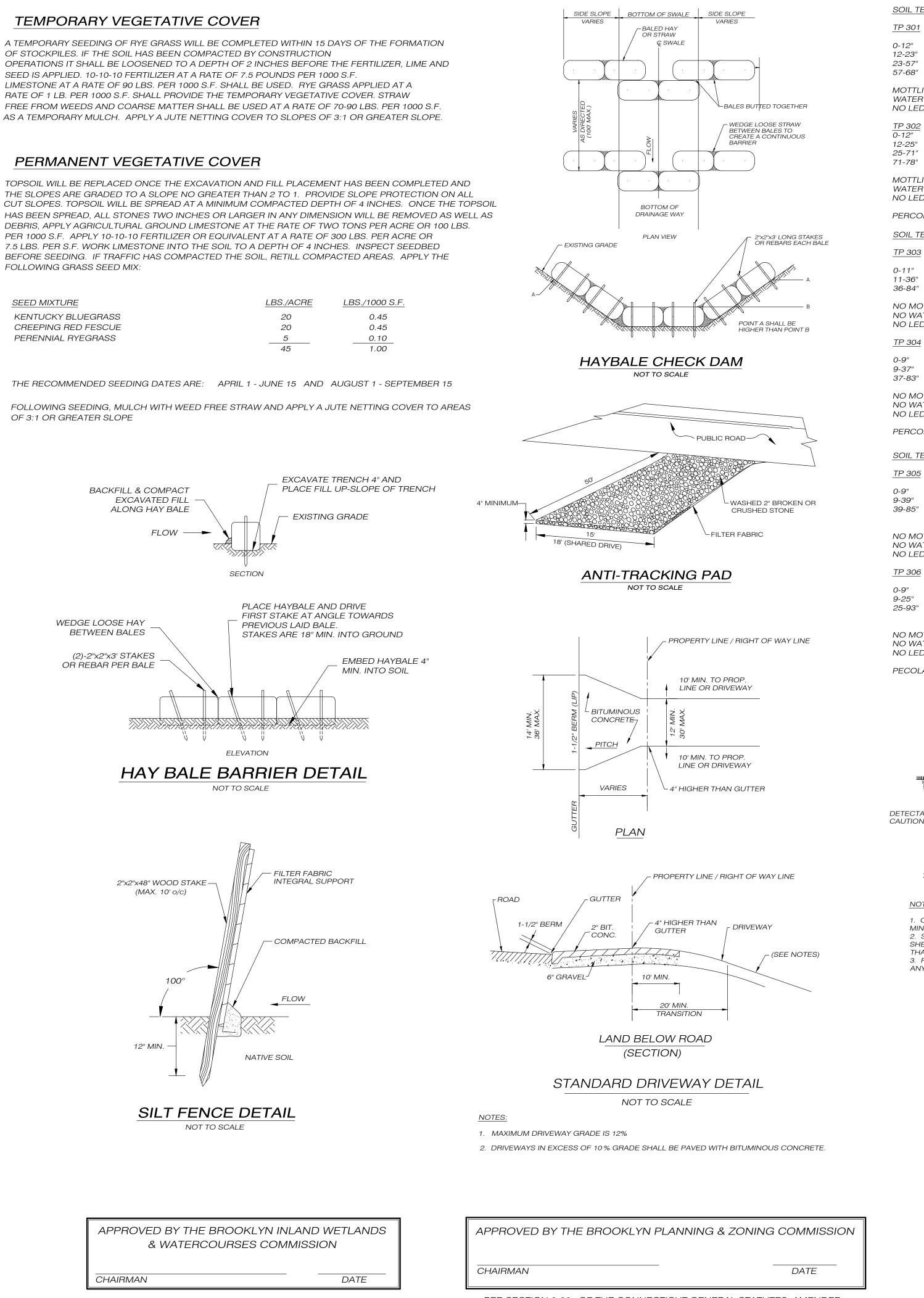
(2)-2"x2"x3' STAKES OR REBAR PER BALE

2"x2"x48" WOOD STAKE-(MAX. 10' o/c)



12" MIN.

CHAIRMAN



PER SECTION 8-26c OF THE CONNECTICUT GENERAL STATUTES, AMENDED, IF ALL PHYSICAL IMPROVEMENTS APPROVAL AUTOMATICALLY EXPIRES REQUIRED BY THIS PLAN ARE NOT COMPLETED BY THAT DATE

SOIL TEST DATA - LOT 17 SEPTIC SYSTEM NOTES - LOT 17 4 BEDROOM HOUSE PERCOLATION RATE: 8 MIN./INCH TOPSOIL EFFECTIVE LEACHING AREA REQUIRED = 577.5 SF FINE SANDY LOAM DEPTH TO RESTRICTIVE LAYER = 23"MOTTLED FINE SANDY LOAM MLSS: $26(HF) \times 1.0(PF) \times 1.75(FF) = 45.5'$ MLSS GROUNDWATER USE STANDARD 12" DEEP x 48" WIDE LEACHING TRENCHES MOTTLING @ 23' MAXIMUM DEPTH INTO GRADE: 5" WATER @ 57" EFFECTIVE LEACHING AREA OF TRENCH = 3.0 SF/LF NO LEDGE LENGTH OF TRENCH REQUIRED = (577.5 SF)/(3.0 SF/LF) = 192.5 LF USE THREE 65' TRENCHES 8' O/C LSS PROVIDED = 65'TOPSOIL LEACHING AREA PROVIDED = 585 SF FINE SANDY LOAM SEPTIC TANK: 1500 GALLON MOTTLED FINE SNDY LOAM w/ GRAVEL/COBBLES ENGINEERED SYSTEM REQUIRED GROUNDWATER MOTTLING @ 25' WATER @ 71" SEPTIC SYSTEM NOTES - LOT 18 NO LEDGE 4 BEDROOM HOUSE PERCOLATION RATE: 2.35 MIN./INCH PERCOLATION RATE: 8 MIN./INCH AT 18" DEPTH EFFECTIVE LEACHING AREA REQUIRED = 577.5 SF DEPTH TO RESTRICTIVE LAYER = N/ASOIL TEST DATA - LOT 18 MLSS: N/A USE STANDARD 12" DEEP x 48" WIDE LEACHING TRENCHES MAXIMUM DEPTH INTO GRADE: N/A EFFECTIVE LEACHING AREA OF TRENCH = 3.0 SF/LFTOPSOIL USE THREE 65' TRENCHES 8' O/C LOAMY SAND GRAVELLY MED. LOAMY SAND w/ COBBLES/STONES LEACHING AREA PROVIDED = 585 SF SEPTIC TANK: 1500 GALLON NO MOTTLING NO WATER NO LEDGE SEPTIC SYSTEM NOTES - LOT 19 **4 BEDROOM HOUSE** PERCOLATION RATE: 1.23 MIN./INCH TOPSOIL EFFECTIVE LEACHING AREA REQUIRED = 577.5 SF 9-37" LOAMY SAND DEPTH TO RESTRICTIVE LAYER = N/A37-83" GRAVELLY MED. LOAMY SAND w/ COBBLES/STONES MLSS: N/A USE STANDARD 12" DEEP x 48" WIDE LEACHING TRENCHES NO MOTTLING MAXIMUM DEPTH INTO GRADE: N/A NO WATER EFFECTIVE LEACHING AREA OF TRENCH = 3.0 SF/LF NO LEDGE LENGTH OF TRENCH REQUIRED = (577.5 SF)/(3.0 SF/LF) = 192.5 LF PERCOLATION RATE: 2.35 MIN./INCH @ 18" DEPTH USE THREE 65' TRENCHES 8' O/C LEACHING AREA PROVIDED = 585 SF SEPTIC TANK: 1500 GALLON SOIL TEST DATA - LOT 19 SOIL TESTING PERFORMED 1/17/2020 BY N.D.D.H. TOPSOIL FILE NO. 20000155 GRAVELLY FINE SANDY LOAM VERY GRAVELLY/COBBLY LOAMY MED. SAND w/ STONES/BOULDERS NO MOTTLING NO WATER NO LEDGE – NO.4 OR NO. 6 STONE TOPSOIL GRAVELLY FINE SANDY LOAM VERY GRAVELLY/COBBLY LOAMY MED. SAND w/ STONES/BOULDERS NO MOTTLING NO WATER NO LEDGE FILTER FABRIC-5050 6" PERF. PECOLATION RATE = 1.23 MIN./INCH AT 24" DEPTHPVC AA PIPE-UNDERDRAIN TRENCH FINISHED GRADE -NOT TO SCALE - SUITABLE BACKFILL 24" MIN. 📽 MATERIAL DETECTABLE 36" MAX OR CAUTION TAPE-AS REQ'D BY UTILITY CO. LOAM & SEED -- MODIFIED RIPRAF 3" MIN. CONDUIT 12" (ELEC., TELE., CTV) NOTES: 1. OSHA STANDARDS REQUIRE THAT SPOILS BE PLACED 24" MIN. FROM EDGE OF TRENCH. 2. SUITABLE BACKFILL SHALL NOT CONTAIN ASH, CINDER, SHELL, FROZEN MATERIAL, LOOSE DEBRIS OR STONES LARGER THAN 2" MAX. DIMENSION. MODIFIED RIPRAP SWALE 3. FUEL OR WATER LINES SHALL BE NO CLOSER THAN 18" IN NOT TO SCALE ANY DIRECTION. UTILITY TRENCH DETAIL NOT TO SCALE KINGSWOOD ESTATES SURVEYING • MAPPING • PLOT PLAN SUBDIVISION OF LAND EROSION CONTROL PLAN & CONSTRUCTION DETAILS PREPARED FOR DAVID P. BELL NANCY M. BELL email: pcsurvey@snet.net CHURCH STREET 63 SNAKE MEADOW RD Killingly Engineering KILLINGLY, CT 06239 BROOKLYN, CONNECTICUT 860 774 6230 Associates 114 Westcott Road P.O. Box 421 Dayville, Connecticut 06241 DATE: APRIL 2020 SHEET NO: 5 OF 5 SCALE: 1'' = AS NOTEDREVISED: 860 779 7299

JOB NO: 18016 F.B. NO:

DRAWN BY: P.A.T. MAP NO:

NORMAND THIBEAULT, JR., P.E. No. 22834 DATE

PLANNING AND ZONING COMMISSION TOWN OF BROOKLYN CONNECTICUT

Received Date	Application # SD
APPLICATION FOR SUBDIVISO	Check # N/RESUBDIVISION
Name of Applicant David & Nancy Bell	Phone860 774 3838
Mailing Address PO Box 358, Brooklyn, CT 06234	
Applicants Interest in the Property <u>owner</u>	
Property OwnerSAME	Phone
Mailing Address	
Name of Engineer/Surveyorpc survey associates, Ilc / Kill	ingly Engineering Associates
Address 63 Snake Meadow Road, Killingly, CT 06239	ingly Engineering Associates
Contact Person Paul A. Terwilliger, LS	
Contact Person	
Name of Attorney	
Address	
Phone Fax	
C. L. division	
Subdivision Re subdivision	
Property location Church Street	25.56 Acres to be Divided 25.56
Map # Lot # Zone_RA Total Acres	
Number of Proposed Lots3 Length of New Road	Proposed
Sewage Disposal: Private Public	d hu Caption 11 4 2
Note: Hydrological report require	
Length of new Sewer proposed: Sanitary <u>n/a</u> Water: Private <u>X</u> Public	
volter. Private Public	-
Is parcel located within 500 feet of an adjoining Town? <u>no</u>	
The following shall accompany the application when required:	
4.2.2 Fee \$ State (\$60.00) 4.2.3 Sanita	ry Report 4.2.5, 3 copies of
plans	
4.2.4 Application/ Report of Decision from the Inland Wetland	s Com. & the Conservation Com.
4.2.6 Erosion & Sediment Control Plans	
4.2.7 Certificate of Public Convenience and Necessity	
4.2.8 Applications filed with other Agencies	
The summer and explicant benchy event the Decide Blemmine en	d Zanina Commission the Board of Salastman
The owner and applicant hereby grant the Brooklyn Planning and Authorized Agents of the Planning and Zoning Commission or Be	
property to which the application is requested for the purpose	
regulations and the Subdivision regulations of the Town of Brown	and the second se
regulations and the Subdivision regulations of the 10wh of Brod	экіуп
Applicant: X Almic Dell	Date 7-6-20
Owner: X Durch. Duck	Date 7- 6 - 20

*Note: All consulting fees shall be paid by the applicant



Northeast District Department of Health 69 South Main Street, Unit 4 Brooklyn, CT 06234 Phone: 860-774-7350 / Fax: 860-774-1308 / web: <u>www.nddh.org</u> OFFICE HOURS: Monday - Friday, 7:00 a.m. - 4:00 p.m.

FILE APPLICATION "B"

(Site Investigation, Septic Plan Review & Permit Application)

PROPERTY LOCATION				
TOWN: BROOKLYN	STREET #:	STREET:	CHURCH STREET	
ASSESSOR'S MAP# 35 BLOCK#	LOT#_	4D	EV.LOT#	UTILITY POLE#
PURPOSE OF APPLICATION: (Check all the	1at apply)			
SITE INVESTIGATION: PLAN REV	VIEW: <u>X</u> PE	RMIT:		
NO. OF LOTS: 3 SIZE OF LOT(s): _	2 - 16 AC	NO. OF	BEDROOMS: <u>4</u>	WATER SUPPLY: Well pr City
RESIDENTIAL X or COMMERCIA	JL:	SUBDIVISION	Yes No If yes	s, is it: NEW or EXISTING
NEW CONST: X or REPAIR:		SUBDIVISION	NAME:	
LOT SPLIT: Yes No Are there any ease	ments?			
YEAR BUILT:				
OWNER INFORMATION LEGAL OWNER: DAVID & NANCY BI	ELL			
MAILING ADDRESS: PO BOX 358				
TOWN: BROOKLYN	STATE: CT		34 EMAIL:	
HOME PHONE: 860 774 3838 W	ORK PHONE:	- 92 - 55	CELL PHONE:	15
APPOINTED AGENT FOR OWNER: PC	SURVEY ASSOC	IATES	AGENT ADDRESS	63 SNAKE MEADOW RD
TOWN: KILLINGLY	STATE: CT Z	ZIF: 06239	AGENTS PHONE:	860 774 6230
SIGNATURE OF AGENT:				DATE:
INSTALLER:	IN	ISTALLER ADI	DRESS:	45
SIGNATURE OF LEGAL PROPERTY OWNER	:			DATE:

PERSON TO CONTACT TO SCHEDULE FIELD TESTING:

Any misrepresentation by the applicant on this form will cause this application to become void and render any fees paid nonrefundable. In the event of application withdrawal by the applicant, NDDH reserves the right to retain a non-refundable processing fee. No services will be rendered until payment is received. Do not fax, return by mail.

Approval to Construct Permits are issued to a specific homeowner and installer, if either were to change, the permit will be voided and re-issued with the new licensed installer or home owner. Applicable permit fee will be assessed for re-issuance of the permit.

PHONE:

===

NDDH USE ONLY - FILE#_

Site Investigation Fee:	Receipt#	Check#	Date:
Add'l Testing Fee:	Receipt#	Check#	Date:
Add'l Testing Fee:	Receipt#	Check#	
Plan Rev. Fee:	Receipt#	Check#	Date:
1 st Revision Fee:	Receipt#	Check#	
2nd Revision Fee:	Receipt#	Check#	Date:
Septic Permit Fee:	Receipt#	Check#	
Standpipe Monitoring:	Receipt#	Check#	Date:
Consultation Fee:	Receipt#	Check#	Date:
Other Fee:	Receipt#	Check#	Date:

P:\Master Forms\File_App_B_and_Consent_REV030112.doc



Northeast District Department of Health 69 South Main Street, Unit 4 Brooklyn, CT 06234 Phone - 860-774-7350 / Fax – 860-774-1308 www.nddh.org

LETTER OF CONSENT

_____ (DATE)

To Whom It May Concern:

I, DAVID BELL	, legal property owner of:
---------------	----------------------------

Street: CHURCH STREET , Town: BROOKLYN

Map #: _____, Block #: _____, Lot #: ____, Dev Lot N#:_____,

As recorded in the Town Assessor's Office, do hereby authorize :

PC SURVEY ASSOCIATES, LLC

to act as my agent and grant permission to apply for:

1	\sim	Soil Testing
· ·	X	Soli resulty

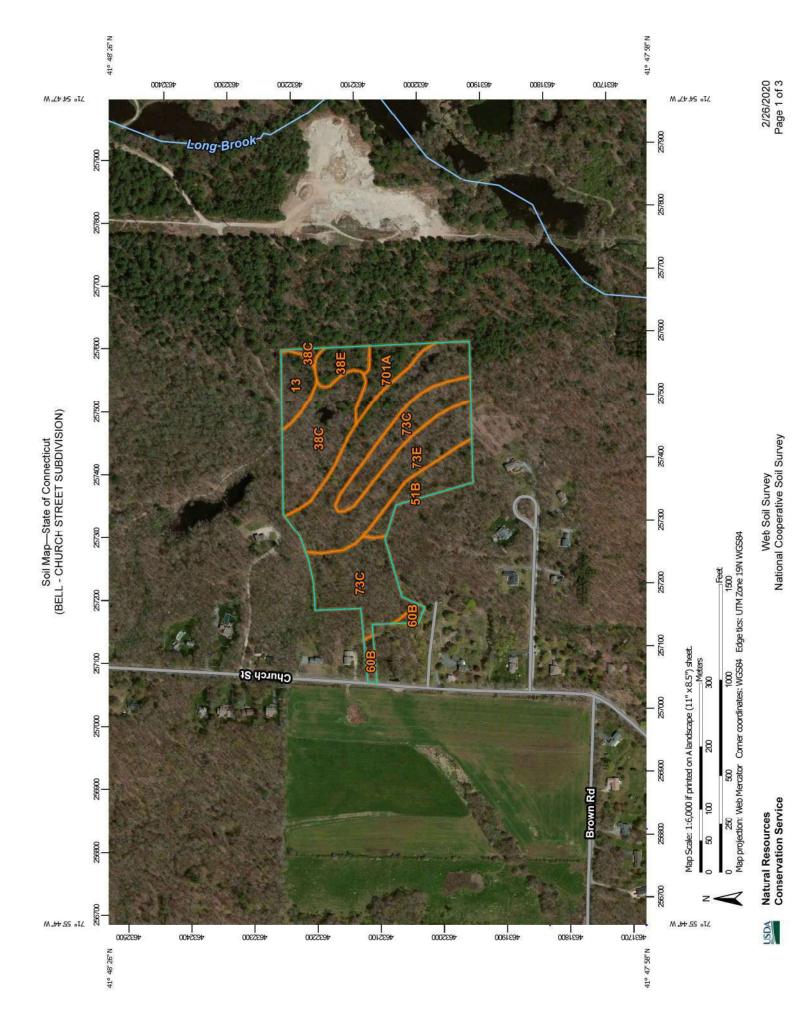
2. _____ Permit to Construct or Repair a Septic System

3. X Other: PLAN REVIEW

In evaluating this application, I realize that the Northeast District Department of Health has relied on information provided by the applicant or agent. If such information subsequently proves to be false, deceptive, incomplete and/or inaccurate, service will be suspended and any permits issued will be revoked. I understand that a permit to construct is issued to a specific CT Licensed Installer, is the property of the installer, and is not transferable.

The undersigned swears that the information supplied in the completed application is accurate to the best of his/her knowledge and belief.

Signature of Legal Property Owner	Date	Telephone #
Signature of Appointed Agent	Date	Telephone #
Letter of Consent REV062207.doc		



Soil Map—State of Connecticut (BELL - CHURCH STREET SUBDIVISION)

MAP INFORMATION	The soil surveys that comprise your AOI were mapped at	1.12,000.	Please rely on the bar scale on each map sheet for map		Source of Map: Natural Resources Conservation Service	Web Soil Survey URL: Coordinate System: Web Mercator (EPSG:3857)	Mana from the Web Cail Gurana are based on the Web Marater	projection, which preserves direction and shape but distorts	distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more	accurate calculations of distance or area are required.	This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.	Soil Survey Area: State of Connecticut		Soil map units are labeled (as space allows) for map scales	1.50,000 or larger.	Date(s) aerial images were photographed: Apr 14, 2011—Aug 27, 2016	The orthophoto or other base map on which the soil lines were	compiled and digitized probably differs from the background	shifting of map unit boundaries may be evident.							
SEND	Spoil Area	Stony Spot	M Very Stony Spot		Viet Spot	△ Other	Special Line Features	Water Features	Streams and Canals	Transportation Rails	Interstate Highways	US Routes	Major Roads	Local Roads	Background	Aerial Photography										
MAP LEGEND	Area of Interest (AOI)	Area of Interest (AOI)		Soil Map Unit Polygons	Soil Map Unit Lines	Soil Map Unit Points	Coorial Doint Foattwoo		hi	Clay Spot	Closed Depression	Gravel Pit	Gravelly Spot	Landfill	Lava Flow E	Marsh or swamp	Mine or Quarry	Miscellaneous Water	Perennial Water	Rock Outcrop	Saline Spot	Sandy Spot	Severely Eroded Spot	Sinkhole	Slide or Slip	Sodic Spot
	Area of Int		Soils		1	-	Concise L			Ж	0	ኡ	***	٥	V		¢	0	0	>	+	•••	Û	\$	~	R

٦



Map Unit Legend

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
13	Walpole sandy loam, 0 to 3 percent slopes	1.2	4.8%
38C	Hinckley loamy sand, 3 to 15 percent slopes	4.3	17.3%
38E	Hinckley loamy sand, 15 to 45 percent slopes	1.0	3.9%
51B	Sutton fine sandy loam, 0 to 8 percent slopes, very stony	1.8	7.3%
60B	Canton and Charlton fine sandy loams, 3 to 8 percent slopes	0.5	1.8%
73C	Charlton-Chatfield complex, 0 to 15 percent slopes, very rocky	6.4	25.6%
73E	Charlton-Chatfield complex, 15 to 45 percent slopes, very rocky	8.5	33.9%
701A	Ninigret fine sandy loam, 0 to 3 percent slopes	1.4	5.4%
Totals for Area of Interest		25.1	100.0%

ABUTTERS WITHIN 200' OF SUBDIVISION - BELL, CHURCH STREET

MAP 35, LOT 3 TURNER JOHN F III & SAVOLIS SARAH K 92 CHURCH ST BROOKLYN CT 6234

MAP 35, LOT 4-16 BESSETTE STEVEN W & ADRIENNE L 28 MALBONE LN BROOKLYN CT 6234

MAP 35, LOT 7 F C R REALTY LLC 110 DAY ST BROOKLYN CT 6234

MAP 35, LOT 2 ENNIS JOHN F & EGGERS FREDERICK S 289 PROVIDENCE RD BROOKLYN CT 06234-1818

MAP 35, LOT 4-4 BURKART THOMAS & SUSAN PO BOX 787 BROOKLYN CT 06234-0356 MAP 35, LOT 4-15 BRAIS ERIC V & SIMONNE D 30 MALBONE LN BROOKLYN CT 6234

MAP 35, LOT 4-14 GALLAGHER MICHELLE L & BILLY JAY 26 MALBONE LN BROOKLYN CT 6234

MAP 35, LOT 4-9 WILLIAMS DONALD E JR & LAURA L 41 MALBONE LN BROOKLYN CT 6234

MAP 35, LOT 4-3 BELL DAVID P & NANCY M P.O. BOX 358 BROOKLYN CT 06234-0844

MAP 35, LOT 4-5 MURRAY DANIEL & RAINVILLE JESSIE 124 CHURCH ST BROOKLYN CT 6234 MAP 35, LOT 7-1 BRENNAN MICHELE 132 CHURCH ST BROOKLYN CT 6234

MAP 35, LOT 7-8 GOYETTE PAMELA J & SCHOBER ANDREA E 136 CHURCH ST BROOKLYN CT 6234

MAP 35, LOT 4-8 GREENE KENNETH N 38 MALBONE LN BROOKLYN CT 06234-2535

MAP 41, LOT 6 F C R REALTY LLC 110 DAY ST BROOKLYN CT 6234

SUBJECT PROPERTY MAP 35, LOT 4 BELL DAVID P & NANCY M PO BOX 358 BROOKLYN CT 6234

COMMON DRIVEWAY AND MAINTENANCE AGREEMENT

WHEREAS, David P. Bell and Nancy M. Bell are the owners of three parcels or lots of land (the "Lots") situated in the Town of Brooklyn, County of Windham and State of Connecticut, shown and designated as Lots 17, 18 and 19 on a map entitled, "KINGSWOOD ESTATES - SUBDIVISION MAP PREPARED FOR DAVID P. BELL & NANCY M. BELL – CHURCH STREET - BROOKLYN, CONNECTICUT – SCALE: 1"=60° – DATE: APRIL 2020 – SHEET 2 OF 5 – PC SURVEY ASSOCIATES, LLC – 63 SNAKE MEADOW RD, KILLINGLY, CT 06239", filed in the office of the Brooklyn Town Clerk, to which map reference is hereby made for a more particular description and location of said premises; and

WHEREAS, the above Lots are to be served by a single common driveway to be located within a Right-of-Way located on Lot 19 as depicted on the above referenced map, and more particularly bounded and described as follows:

Commencing at a point in the easterly line of Church Street at the southwesterly corner of the herein described Right-of-Way and the northwesterly corner of other land now or formerly of David P. & Nancy M. Bell, said point being located 509 feet, more or less, southerly of an iron pin at the westerly end of a stone wall in the easterly line of Church Street at the northwesterly corner of Lot 5 and the southwesterly corner of land now or formerly of Kyle M. Timoteo;

Thence Northeasterly, 33.23 feet along a curve to the right having a radius of 25.00 feet to a point, bounded southeasterly by other land now or formerly of David P. & Nancy M. Bell;

Thence S 87° 35' 50" E, 363.36 feet to a point, bounded southerly by other land now or formerly of David P. & Nancy M. Bell, Parcel "A" and Lot 17;

Thence N 70° 51' 39" E, 197.61 feet to a point;

Thence S 75° 14' 47" E, 129.87 feet to a point at the southeasterly corner of the herein described Right-of-Way, the last two courses bounded southerly by Lot 17;

Thence N 29° 49' 50" E, 51.78 feet across the access strip of Lot 19 to a point at an angle in the southerly line of Lot 18;

Thence N 75° 14' 47" W, 158.57 feet to a point;

Thence S 70° 51' 39" W, 203.34 feet to a point at the southeasterly corner of land now or formerly of Thomas & Susan Burkart, the last two courses bounded northerly by Lot 18 Thence N 87° 35' 50" W, 330.31 feet to a point;

Thence Northwesterly, 44.84 feet along a curve to the right having a radius of 25.00 feet to a point in the easterly line of Church Street, the last two courses bounded northerly and northeasterly by land now or formerly of said Burkart;

Thence S 15° 09' 29" W, 55.96 feet along the easterly line of Church Street to a point; Thence S 16° 14' 16" W, 46.31 feet along the easterly line of Church Street to the point of beginning; and

WHEREAS, said David P. Bell & Nancy M. Bell wish to set forth the respective rights and obligations of all current and future owners of said Lots;

NOW THEREFORE, it is hereby resolved that:

 The owners of the Lots shall share in the costs of constructing, repairing and maintaining the driveway in good condition and repair in the following percentages: Lot 17: 33-1/3%, Lot 18: 33-1/3% and Lot 19: 33-1/3%. Maintenance and repair shall include but not be limited to, replacement, restoration and repair of the driveway surface including but not limited to bituminous concrete, concrete, tar, asphalt, stone and gravel, snow and ice removal, removal of fallen trees and other obstructions, etc. Each Lot owner agrees to provide the owners of the other Lot with copies of bills, invoices or statements relating to the costs of repairing and maintaining the driveway.

(2) Items of repair and/or maintenance to be performed shall be determined as agreed upon by the owners of the Lots, provided however, that neither Lot owner may unreasonably withhold his or her consent of agreement to items of repair or maintenance reasonably necessary to keep the driveway in good and passable condition and repair and repair of public utilities if required.

(3) Each Lot owner shall be liable to contribute to the cost in the percentages set forth in paragraph number one (1) above, such amounts to be paid promptly upon completion of such repair or maintenance item, and after presentment of a copy of the invoice from the contractor performing the work.

(4) Notwithstanding the foregoing obligation to share equally in maintenance and repair expenses, in the event that the owner of either Lot, or such owner's agents, contractors, employees or guests shall damage the driveway by construction activities related to such owner's Lot or by negligence, reckless or intentional act, then such owner shall promptly repair such damage and restore the driveway to its prior condition, at his/her sole expense.

(5) It is mutually agreed that the owners of Lots 17, 18 and 19 shall have the right to install underground public utilities along said driveway at their sole expense, and the owners of Lots 17, 18 and 19 are hereby granted ingress and egress over said driveway and access is hereby granted over said driveway to any utility company (including, but not limited to electric, telephone, gas, water, sewer, cable TV) for installation / maintenance / repair of such services to any Lot. Any land disturbed by the exercise of these rights shall be restored, by the party exercising the right, to the condition it was prior to such entry.

(6) No owners of the Lots, or an owner's agent(s) or guest(s) shall in any way at any time obstruct the driveway, park vehicles or allow any to be parked on the driveway or in any other way prevent or hinder free passage by an owner, or an owner's agent(s) or guest(s) over such portion of the driveway Right-of-Way which serves each owners' property.

(7) The owners of Lots 17, 18 and 19 and such future owners of Lots 17, 18 and 19, covenant and agree to indemnify, defend and hold each other harmless against all claims, demands, loss, damage, liabilities and expenses and all suits, actions and judgments (including, but not limited to costs and reasonable attorneys' fees) arising out of or in any way related to their failure to maintain in a safe condition the easements granted and created hereunder. The owners of Lots 17, 18 and 19 shall give prompt and timely notice of any claim or suit or action commenced against the other party which in any way would result in indemnification hereunder.

(8) In the event of failure by any owner of Lots 17, 18 and 19 to perform, fulfill or observe any agreement herein performed, fulfilled or observed by it, which failure shall continue for thirty (30) days, or in situations involving potential danger to the health and safety of persons in, on or about Lots 17, 18 and 19 or any portion or part thereof, in each case after written notice specifying such, the other party may, at its election, cure such failure or breach for and on behalf of the defaulting party, and any amount which the party so electing shall expend for such purpose, or which shall otherwise be due by either party to the other hereunder, shall be paid to the party to whom due upon demand, without contest upon delivery of its invoice, together with interest thereon at the lower of (i) the rate of 10% per annum, or (ii) the maximum rate permissible from time to time

under applicable law, from the date of expenditure or the date when same shall have become due to the date of payment thereof in full.

(9) This agreement shall be binding on the owners of the Lots, their heirs, successors and assigns and inure to their respective benefit, shall be appurtenant to, run with, and benefit Lots 17, 18 and 19 as shown on the aforementioned map.

(10)Any disagreement between any owner of Lot 17. Lot 18 and/or Lot 19 delineated on the subdivision map referred to in the first recital paragraph of this Declaration with respect to the interpretation or application of these covenants and their obligations hereunder shall be determined by arbitration. Such arbitration shall be conducted upon the request of any land owner of Lot 17, Lot 18 and/or Lot 19 delineated on the subdivision map referred to in the first recital paragraph of this Declaration. Any such arbitration proceeding shall be conducted in accordance with the rules of the American Arbitration Association or as may be mutually agreed. The decision of the arbitrator in any such proceeding shall be final, and judgment upon the arbitration award may be entered in any court of competent jurisdiction. The expense of arbitration proceedings conducted hereunder shall be borne equally by the parties. Notwithstanding the foregoing, the parties agree that any and all disputes less than \$5,000.00 (or the then monetary limit of Small Claims Court) shall be heard in Small Claims Court. In any event, the successful party shall be entitled to recover all costs and expenses incurred including reasonable attorneys' fees to be fixed as a result of Arbitration or by Small Claims Court.

IN WITNESS WHEREOF, David Bell and Nancy M. Bell have hereunto set their hands and seals this day of

PERMANENT CONSERVATION EASEMENT COVENANT

WHEREAS, David P. Bell and Nancy M. Bell are the owners of three parcels or lots of land (the "Lots") situated in the Town of Brooklyn, County of Windham and State of Connecticut, shown and designated as Lots 17, 18 and 19 on a map entitled, "SUBDIVISION MAP PREPARED FOR DAVID P. BELL & NANCY M. BELL – CHURCH STREET, BROOKLYN, CONNECTICUT – SCALE: 1"=60' – DATE: APRIL 2020 – REVISED 7/6/2020 – SHEET 2 OF 5 - PC SURVEY ASSOCIATES, LLC – 63 SNAKE MEADOW RD, KILLINGLY, CT 06239", as filed in the office of the Brooklyn Town Clerk, to which map reference is hereby made for a more particular description and location of said premises; and

WHEREAS, the above Lots are to be SUBJECT TO Conservation Easements as depicted on the above referenced map, and more particularly bounded and described as follows:

CONSERVATION EASEMENT "A"

A certain Conservation Easement over Lot 17 bounded and described as follows:

Beginning at a point in the southerly line of the access strip of Lot 19 at the northwesterly corner of the herein described Conservation Easement, said point being located easterly, 639 feet, more or less, from the easterly line of Church Street as measured along the southerly line of the access strip of Lot 19;

Thence S 75° 14' 47" E, 85.04 feet along the southerly line of the access strip of Lot 19 to a point;

Thence S 45° 05' 33" E, 256.32 feet along the southwesterly line of the access strip of Lot 19 to an iron rod in a stone wall corner;

Thence N 72° 04' 59" W, 270.00 feet to a point;

Thence N 54° 45' 18" W, 58.19 feet to a point;

Thence N 19° 08' 21" E, 67.89 feet to a point;

Thence N 70° 51' 39" E, 66.60 feet to the point of beginning.

CONSERVATION EASEMENT "B"

A certain Conservation Easement over Lot 18, Lot 19, and Parcel "B", bounded and described as follows:

Beginning at a point in the northerly line of the access strip of Lot 19 at the southwesterly corner of the herein described Conservation Easement and the southeasterly corner of land now or formerly of Thomas & Susan Burkart, said point being located easterly, 375 feet, more or less, from the easterly line of Church Street as measured along the northerly line of the access strip of Lot 19;

Thence N 02° 24' 09" E, 170.85 feet to a point in a stone wall, bounded westerly by land now or formerly of Thomas & Susan Burkart;

Thence N 89° 18' 50" E, 85.79 feet along a stone wall to a point;

Thence S 85° 32' 44" E, 35.19 feet along the stone wall to a point at a wall intersection;

Thence S 87° 31' 50" E, 95.66 feet along the stone wall to a point;

Thence S 88° 33' 29" E, 120.18 feet along the stone wall to a point;

Thence N 83° 54' 29" E, 54.30 feet along the stone wall to a point;

Thence N 72° 10' 21" E, 20.23 feet along the stone wall to a point;

Thence N 39° 51' 45" E, 18.49 feet along the stone wall to a point;

Thence N 28° 20' 45" E, 94.12 feet along the stone wall to an iron rod at a wall intersection, the last seven courses bounded northerly and westerly by land now or formerly of Daniel Murray and Jesse Rainville;

Thence S 72° 02' 58" E, 31.45 feet along a stone wall to a point;

Thence S 76° 53' 04" E, 198.36 feet along the stone wall to an iron rod in a wall intersection, the last two courses bounded northerly by land now or formerly of Pamela J. Goyette and Andrea E. Schober;

Thence S 83° 55' 56" E, 52.61 feet to a point;

Thence S 74° 44' 59" E, 121.05 feet to a T-Bar;

Thence S 81° 35' 04" E, 129.66 feet to a point;

Thence S 77° 45' 20" E, 104.23 feet to a point, the last four courses bounded northerly by land now or formerly of FCR Realty, LLC;

Thence S 13° 12' 58" E, 845.54 feet to a point in the southerly line of Lot 19;

Thence S 08° 46' 33" W, 81.51 feet to a point;

Thence N 81° 13' 27" W, 152.81 feet to a point;

Thence N 36° 44' 27" W, 148.13 feet to a point in the southerly line of Lot 19;

Thence N 13° 12' 58" W, 771.00 feet to a point;

Thence N 76° 53' 04" W, 305.25 feet to a point;

Thence S 67° 24' 45" W, 244.97 feet to a point;

Thence S 76° 04' 44" W, 150.00 feet to a point in the northerly line of the access strip of Lot 19; Thence S 70° 51' 39" W, 203.34 feet along the northerly line of the access strip of Lot 19 to the point of beginning; and

WHEREAS, said David P. Bell and Nancy M. Bell wish to set forth the respective rights, restrictions and obligations of all current and future owners of said Lots;

NOW THEREFORE, it is hereby resolved that:

Lots 17, 18 and 19 shall be conveyed subject to said Conservation Easements and restrictions and covenants set forth in the various paragraphs of this document.

This Conservation Easement ("Easement") shall be deemed to be a covenant that runs with the land and shall be binding upon David P. & Nancy M. Bell, their successors and assigns and all persons claiming through the Grantor in perpetuity.

If the Grantor, its successors and assigns, or any person claiming under them shall violate or attempt to violate any of the covenants herein, it shall be lawful for the Town of Brooklyn, through its Planning and Zoning Commission, to take any and all steps necessary to enforce this Conservation Easement, to prosecute any proceedings at law or in equity against the person or persons violating or attempting to violate any such covenant and either to prevent him or them from so doing and to recover damages incurred, to recover all attorney's fees and costs or any other expenses incurred in enforcing this Conservation Easement or in correcting the results of any violation of said Easement. The Grantor shall include in the conveyance of Lots 17, 18 & 19 the right to enforce this Open Space Easement against any party in violation of any provision of this Conservation Easement. Said conveyance shall contain the language:

"Said premises are conveyed together with the right to enforce the Conservation Easement for purposes of preserving and protecting Open Space that is conveyed as a part of said property, all as provided in said Conservation Easement as of record appears."

To preserve and protect the Easement in perpetuity, the following restrictions on use and activities shall be enforced in perpetuity with respect to said Easement:

- 1. No structure of any kind may be built on said Easement.
- 2. No motorized vehicles shall enter upon or travel across the Easement.
- 3. No dumping shall occur on said Easement.
- 4. No fires shall occur on said Easement.
- 5. No mining or natural resource extraction shall occur on said Easement.
- 6. No harvesting of timber or firewood except as part of a long term management plan prepared by a professional forester and approved in advance by the Brooklyn Conservation Commission shall occur in the easement. No liquidation cuts or clearcutting are allowed on said Easement.
- 7. Lot 19 shall have the right to cross the Easement in one suitable area to access land beyond the easement area.
- 8. The Lot owners shall have the right to use the easement areas located on their respective Lots for passive recreational activities.

Invalidation of any of these covenants by judgment or court order shall in no way affect any of the other provisions which shall remain in full force and effect.

TO HAVE AND TO HOLD the above granted Open Space Easement unto the said Grantee, its successors and assigns forever, to it and its own proper use and behoof.

IN WITNESS WHEREOF, the Grantor has hereunto set its hand and seal onthisday of20

This Confinements and International mail. This Confinement of Mailing provides evidence that mail has been presented to USPS® for mailing. From: PC Survey Associates, LLC 63 Shake Meadow Road Killingly, CT 06239 To: DANIEL MURRAY & JESSIE RAINVILLE 124 CHURCH ST BROOKLYN CT 06234	To pay fee, affix stamps or meter postage here. Postmark Here	This Certificate of Mailing provides evidence that mail has been presented to USPS® for mailing. Certificate Of Mailing provides evidence that mail has been presented to USPS® for mailing. From: PC Survey Associates, LLC Mailing From: PC Survey Associates, LLC Mailing From: PC Survey Associates, LLC 63 Snake Meadow Road Inc To: Thomas & SUSAN BURKART Thomas & SUSAN BURKART Inc To: THOMAS & SUSAN BURKART PC 800X 787 BROOKLYN CT 06234	Certificate Of Mailing	To pay fee, affix stamps or meter postage here. *
PS Form 3817, April 2007 PSN 7530-02-000-9065		PS Form 3817, April 2007 PSN 7530-02-000-9065		
Image: Construct of the second sec	To pay fee, affix stamps or meter postage here.	This Certificate of Mailing provides evidence that mail has been presented to USPS® for mailing. From: PC Survey Associates, LLC	Certificate Of Mailing Med to USPS® for mailing.	To pay fee, affit stamps or meter postage here.
Killingly, CT 06239 FCRREALTYLLC	Postmark Here	63 Snake Meadow Road Killingly, CT 06239 To:		Postmark Here
- 110 DAY ST BROOKLYN CT 06234 - BROOKLYN CT 06234 - PS Form 3817 , April 2007 PSN 7530-02-000-9065		 JOHN F ENNIS & FREDERICK S EGGERS 289 PROVIDENCE RD BROOKLYN CT 06234 		

•

This Certificate Of Mailing POSTAL SERVICE [®] Certificate Of Mailing This Certificate of Mailing provides evidence that mail has been presented to USPS [®] for mailing. This form may be used for domestic and international mail. From: PC Survey Associates, LLC	Certificate Of Mailing presented to USPS® for mailing.	To pay fee, affix stamps or meter postage here.	This Certificate Of Mailing provides evidence that mall has been presented to USPS® for mailing. From: PC SUTVery Associates, LLC	Certificate Of Mailing med to USPS® for mailing.	To pay fee, affix stamps or meter postage here.
63 Snake Meadow Road Killingly, CT 06239			63 Snake Meadow Road Killingly, CT 06239		
To: KENNETH N GREENE 38 MALBONE LN BROOKLYN CT 06234		Postmark Here	To: DONALD E JR & LAURA L WILLIAMS 41 MALBONE LN BROOKLYN CT 06234		Postmark Here
PS Form 3817, April 2007 PSN 7530-02-000-9065	-9065		PS Form 3817, April 2007 PSN 7530-02-000-9065		
WITED STATES Certificate Of Mailing This Certificate of Maling provides evidence that mall has been presented to USPS® for mailing. This form may be used for domestic and international mail. From: pc Surrvey Associates, IJLC	Certificate Of Mailing been presented to USPS® for mailing.	To pay fee, affix stamps or meter postage here.	Image: Construction of the second	Certificate Of Mailing Inted to USPS® for mailing.	To pay fee, affix stamps or meter postage here.
63 Snake Meadow Road Killingly, CT 06239			63 Snake Meadow Road Killingly, CT 06239		
To:	ALLAGHER	Postmark Here	To: STEVEN W & ADRIENNE L BESSETTE S8 MALBONE LN BROOKLYN CT 06234 		Postmark Here
PS Form 3817, April 2007 PSN 7530-02-000-9065	0-9065		PS Form 3811, April 2007 PSN 7530-02-000-9065		

b3 Shake meader b3 Shake meader killingly, CT 06239 killingly, CT 06239 BROOKLYN CT 06234 BROOKLYN CT 06234 PS Form 3817, April 2007 PSN 7530-02-000-9065 PS Form 3817, April 2007 PSN 7530-02-000-9065	Postmark Here 63 Snarke w Killingly, 136 - BROO - BROO - BROO PS Form 3817, April 200 PS Form 3817, April 200 PS Form 3817, April 200 Form may be used for domestic fins form for the form for domestic fins for	pc Survey Associates, Juc. 63 Snake Meadow Road Killingly, CT 06239 Killingly, CT 06239 Family Schurch ST BROOKLYN CT 06234 BROOKLYN CT 06234 BROOKLYN CT 06234 PS Form 3817, April 2007 PSN 7530-02-000-9065 Ectificate of Mailing Provides evidence that mall has been presented to USPS® for mailing The certificate of Mailing provides evidence that mail has been presented to USPS® for mailing Certificate of Mailing provides evidence that mail has been presented to USPS® for mailing From: PC Survey Associates, LLC 63 Snake Meadow Road Killindly. CT 06239 Killindly. CT 06239	Postmark Here
To: JOHN F TURNER III & SARAH K SAVOLIS 92 CHURCH ST 92 CHURCH ST BROOKLYN CT 06234	Postmark Here	Te: ERIC V & SIMONNE D BRAIS BROOKLYN CT 06234 PS Form 3817 , April 2007 PSN 7530-02-000-9065	Postmark Here



NORTHEASTERN CONNECTICUT COUNCIL OF GOVERNMENTS

ENGINEERING PLAN REVIEW PERTAINING TO A 3-LOT SUBDIVISION (Assessor's MAP 35, LOT 4) CHURCH STREET BROOKLYN, CT (July 13, 2020)

The comments contained herein pertain to my review of plans for a 3-lot residential subdivision on Church Street in Brooklyn, Connecticut, consisting of five (5) sheets, prepared for David P. Bell and Nancy M. Bell by PC Survey Associates, LLC and Killingly Engineering Associates, dated April 2020 with revision date of July 6, 2020 (Sheets 1-4). My comments are as follows:

Sheet 2 of 5 – Subdivision Map

- 1. Recommend adding "tangent" lengths to the curve table.
- 2. Recommend placing detectable permanent markers at each corner or angle point of the boundary lines for each Conservation Easement.

Sheet 3 of 5 – Lot Development Areas Subdivision of Land

1. Since there is proposed grading and drainage structures on this plan, it is recommended that the signature block and seal of the professional engineer be added to this plan as well as the Killingly Engineering Associates logo.

Sheet 4 of 5 – Lot Development Plan Subdivision of Land

- 1. Wetland flagging is shown on this plan. However, submitted with the Applicant's wetlands application is a letter from soil scientist Michael Schaefer, dated May 29, 2006 regarding the wetland features that he delineated on May 27, 2006. Since the time of delineating the wetlands, several lots in the Kingswood Estates subdivision have been developed with single-family homes. Presently, it is unknown if any lot development has had an impact on the 14 year old wetland delineation, as such, I do not believe it is unreasonable to have the wetlands delineated again to see if any of their boundaries have changed. A signature block for the wetlands scientist should be placed on the plan.
- 2. Recommend using flared end sections on inlet and outlet of 12" cross culvert.

3. A signature block is missing for the professional engineer responsible for all engineering aspects of the plan.

Sheet 5 of 5 – Erosion Control Plan and Construction Details

- 1. In the "Underdrain Trench" detail, how are the perforations to be oriented up or down? Also, the current Connecticut Department of Transportation publication is Form 818, thus the note should be corrected.
- 2. A signature block is missing for the professional engineer responsible for all engineering aspects of the plan.
- 3. In the "Residential Driveway Detail," it should be noted that side slopes in cut or fill shall be no steeper than 3H:1V and a reference be made to the Brooklyn Public Improvement Specifications.

General Comment

1. Drainage calculations have not been submitted for review for the 12" cross culvert and the trapezoidal drainage swale.

7/13/2020 By: NECCOG Regional Engineer Syl Pauley, Jr., P.E

PLANNING AND ZONING COMMISSION TOWN OF BROOKLYN

CONNECTICO	
Received Date	Application # SD
	<i>Check #</i>
APPLICATION FOR SUBDIVISO	N/RESUBDIVISION
Name of Applicant David & Nancy Bell	Phone 860 774 3838
Mailing AddressPO Box 358, Brooklyn, CT 06234	
Applicants Interest in the Property owner	
Applicants interest in the tropertyowner	
Property Owner SAME	Phone
Mailing Address	
Name of Engineer/Surveyorpc survey associates, Ilc / Killir Address63 Snake Meadow Road, Killingly, CT 06239	
Contact Person Paul A. Terwilliger, LS	_Phone860 774 6230
Name of Attorney	
Address	
Phone Fax	
Subdivision Re subdivision	
Property location 131 Prince Hill Road	
Map #34 Lot #52 Zone_RA Total Acres	6 Acres to be Divided 6
Number of Proposed Lots <u>3</u> Length of New Road	d Proposedn/a
Sewage Disposal: Private Public	
Note: Hydrological report requir	
Length of new Sewer proposed: Sanitary	
Water: Private Public	
Is parcel located within 500 feet of an adjoining Town?	
The following shall accompany the application when required:	
4.2.2 Fee \$ State (\$60.00) 4.2.3 Sanita	ary Report 4.2.5. 3 copies of
plans	
4.2.4 Application/ Report of Decision from the Inland Wetland	is Com & the Conservation Com
4.2.6 Erosion & Sediment Control Plans	
4.2.7 Certificate of Public Convenience and Necessity	
4.2.8 Applications filed with other Agencies	
The owner and applicant hereby grant the Brooklyn Planning an	d Zoning Commission, the Board of Selectman,
Authorized Agents of the Planning and Zoning Commission or B	oard of Selectman, permission to enter the
property to which the application is requested for the purpose	of inspection and enforcement of the Zoning
regulations and the Subdivision regulations of the Town of Bro	oklyn
() Quint) 2000	N+ 7/1-120

Applicant: <u>X</u>	Clanc	1. Dell	Date_ 7/6/20	2
Owner: <u>X</u>	Alend)	Bel	Date_7/6/22	2

*Note: All consulting fees shall be paid by the applicant



Northeast District Department of Health 69 South Main Street, Unit 4 Brooklyn, CT 06234 Phone: 860-774-7350 / Fax: 860-774-1308 / web: <u>www.nddh.org</u> OFFICE HOURS: Monday - Friday, 7:00 a.m. - 4:00 p.m.

FILE APPLICATION "B"

(Site Investigation, Septic Plan Review & Permit Application)

PROPERTY LOCATION	
TOWN: BROOKLYN STREET #: 131 STREET: PRINCE HILL ROA	۶D
ASSESSOR'S MAP# 34 BLOCK# LOT# 52 DEV.LOT#	_UTILITY POLE#
PURPOSE OF APPLICATION: (Check all that apply)	
SITE INVESTIGATION: PLAN REVIEW: X PERMIT:	
NO. OF LOTS: <u>3</u> SIZE OF LOT(s): <u>2 AC</u> NO. OF BEDROOMS: <u>4</u>	_WATER SUPPLY: Well pr City
RESIDENTIAL X or COMMERCIAL: SUBDIVISION: Yes No If ye	s, is it: NEW or EXISTING
NEW CONST: X or REPAIR: SUBDIVISION NAME:	
LOT SPLIT: Yes No Are there any easements?	
YEAR BUILT:	
OWNER INFORMATION LEGAL OWNER: DAVID & NANCY BELL	
MAILING ADDRESS: PO BOX 358	
TOWN: BROOKLYN STATE: CT ZIP: 06234 EMAIL:	
HOME PHONE: 860 774 3838 WORK PHONE:CELL PHONE:	10
APPOINTED AGENT FOR OWNER: PC SURVEY ASSOCIATES AGENT ADDRESS	63 SNAKE MEADOW RD
TOWN: KILLINGLY STATE: CT ZIP: 06239 AGENTS PHONE:	860 774 6230
SIGNATURE OF AGENT:	DATE:
INSTALLER:INSTALLER ADDRESS:	
SIGNATURE OF LEGAL PROPERTY OWNER	DATE

PERSON TO CONTACT TO SCHEDULE FIELD TESTING:

Any misrepresentation by the applicant on this form will cause this application to become void and render any fees paid nonrefundable. In the event of application withdrawal by the applicant, NDDH reserves the right to retain a non-refundable processing fee. No services will be rendered until payment is received. Do not fax, return by mail.

Approval to Construct Permits are issued to a specific homeowner and installer, if either were to change, the permit will be voided and re-issued with the new licensed installer or home owner. Applicable permit fee will be assessed for re-issuance of the permit.

PHONE:

===

NDDH USE ONLY - FILE#_

Site Investigation Fee:	Receipt#	Check#	Date:
Add'l Testing Fee:	Receipt#	Check#	Date:
Add'l Testing Fee:	Receipt#	Check#	
Plan Rev. Fee:	Receipt#	Check#	Date:
1st Revision Fee:	Receipt#	Check#	
2 nd Revision Fee:	Receipt#	Check#	Date:
Septic Permit Fee:	Receipt#	Check#	Date:
Standpipe Monitoring:	Receipt#	Check#	Date:
Consultation Fee:	Receipt#	Check#	
Other Fee:	Receipt#	Check#	Date:

P:\Master Forms\File_App_B_and_Consent_REV030112.doc



Northeast District Department of Health 69 South Main Street, Unit 4 Brooklyn, CT 06234 Phone - 860-774-7350 / Fax – 860-774-1308 www.nddh.org

LETTER OF CONSENT

_____ (DATE)

To Whom It May Concern:

I, DAVID BELL	, legal property owner of:
---------------	----------------------------

Street: 131 PRINCE HILL ROAD , Town: BROOKLYN

Map #: <u>34</u>, Block #: _____, Lot #: <u>52</u>, Dev Lot N#:____,

As recorded in the Town Assessor's Office, do hereby authorize :

PC SURVEY ASSOCIATES, LLC

to act as my agent and grant permission to apply for:

107		9229 0125 0160 60789
1	V	Soil Testing
	A	OUI resulty

2. Permit to Construct or Repair a Septic System

3. X Other: PLAN REVIEW

In evaluating this application, I realize that the Northeast District Department of Health has relied on information provided by the applicant or agent. If such information subsequently proves to be false, deceptive, incomplete and/or inaccurate, service will be suspended and any permits issued will be revoked. I understand that a permit to construct is issued to a specific CT Licensed Installer, is the property of the installer, and is not transferable.

The undersigned swears that the information supplied in the completed application is accurate to the best of his/her knowledge and belief.

Signature of Legal Property Owner	Date	Telephone #
Signature of Appointed Agent	Date	Telephone #
Letter of Consent REV062207.doc		

B100a / Change in Use Application INSTRUCTIONS

This form is used to conform to the State of Connecticut Public Health Code, Section 19-13-B100a, which governs building conversions and/or changes in use to existing structures, building additions, garages/accessory structures, swimming pools and sewage disposal area preservation.

You are filling out this form to provide the local health department with important information to assure that you:

- Do not build any structures on top of an existing septic system
- Conform to the required separating distances and maintain your property according to the Connecticut Public Health Code

In addition to filling out this form, you will also have to submit the following with your application:

- If your home was built prior to 1985, please have your deed or the previous owners name available. This information can be obtained from your Town Clerk.
- A plot plan drawn to scale (Example: linch = 20 feet) which shows the location of the house, well, sewer line, and closest property lines. Be sure to indicate distances between each item.
- □ A written description of the proposed addition, accessory structure or pool.
- If proposing an addition to an existing structure, please provide a sketch of the existing floor plan and a sketch of the proposed floor plan change.

Exemptions

You are not required to file a B100a Application for the following:

- Accessory buildings or small sheds which will sit directly on pressure treated plywood or concrete blocks and do not require the pouring of a concrete slab, frost protected footings, sonar tubes or any other foundation that would disturb the soil.
- Anchoring kits that prevent uplift due to winds which are used for temporary structures like removable carports. Anchors should not penetrate the septic system. (Please note, anchoring kits that require the pouring of a concrete slab or permanent footings **do** require a B100a application.)

In the event of application withdrawal by the applicant, NDDH reserves the right to retain a non-refundable processing fee.

In order to avoid unnecessary delays, please be sure to complete the form in its entirety and submit all required information and fees. Do not fax, return by mail. For additional assistance, please contact us:



Northeast District Department of Health

69 South Main Street, Unit 4 Brooklyn, CT 06234

Phone: 860-774-7350 / Fax: 860-774-1308 / www.nddh.org

N:\Master Forms\B100a_App_REV081011.doc

Northeast District Department of Health



69 South Main Street, Unit 4 Brooklyn, CT 06234 Phone - 860-774-7350 / Fax - 860-774-1308 <u>www.nddh.org</u> Office Hours: Monday - Friday 7:00 am – 4:00 pm

B100a /Change in Use Application

To conform to PHC 19-13-B100a – Building Conversions/Changes in Use, Building Additions, Garages/Accessory Structures, Swimming Pools, Sewage Disposal Area Preservation (See Reverse Side for Instructions.)

Town: BROOKLYN Street #: 131 Street: PRINCE HILL R				
Assessor's Map: <u>34</u> Block: <u>Lot: 52</u> Dev. Lot :	Lot Size: <u>o A</u> C			
Legal Owner: DAVID & NANCY BELL				
Mailing Address: PO BOX 358				
Town: BROOKLYN State: CT Zip: 06	234			
Work Phone: Home Phone: 860 774 3838 Cell Phone:				
Email Address:				
Year Built: <u>1800's</u> No. of Bedrooms: <u>3</u>				
Proposal for Change:SUBDIVISION				
Agent for Owner: PC SURVEY ASSOCIATES, LLC				
Agent's Mailing Address: 63 SNAKE MEADOW ROAD				
Town: KILLINGLY State: CT Zip: 062	239			
Agent's Phone Number: 860 774 6230				
Signature of Legal Property Owner: Date:				
PROPERTY OWNER: By signing above, you certify that the information provided is a true	and accurate			
description of the addition, accessory structure, or pool.				
No services will be rendered until payment is received. Do not fax, return by mail. In the event of application withdrawal by the applicant, NDDH reserves the right to retain a non-refundable processing fee.				
NDDH Use Only File #:B100a/Change in Use Fee:				
Name ID #:				

ABUTTERS WITHIN 200' OF SUBDIVISION - BELL , PRINCE HILL ROAD

MAP 34, LOT 2 RZEZNIKIEWICZ RUD 12 GORMAN RD	OLPH		MAP 34, LOT 2A PERRY BRITTANY 158 MAIN ST - PO	Provenene Calabarae de		MAP 34, LOT 3A SIMONZI MARK 25 TOWER HILL RI	D	
BROOKLYN	CT	06234-1804	PUTNAM	CT	06260-0682	BRIMFIELD	MA	01010-9756
MAP 34, LOT 3B			MAP 34, LOT 4			SUBJECT PROPER	RTY MAP 3	34, LOT 52
148 PRINCE HILL LLC			ENNIS JOHN F & E	GGERS F	REDERICK S	BELL DAVID P & N	ANCY M	
59 SOUTH ST			289 PROVIDENCE	RD		P.O. BOX 358		
BROOKLYN	СТ	6234	BROOKLYN	СТ	06234-1818	BROOKLYN	СТ	06234-1521
MAP 34, LOT 51			MAP 34, LOT 53			MAP 34, LOT 3		
RZEZNIKIEWICZ RUD	OLPH	& RITA N (TIC)	ENNIS JOHN F & E	GGERS F	REDERICK S	CIL REALTY INCOR	RPORATE	D
12 GORMAN RD			289 PROVIDENCE	RD		157 CHARTER OAK	AVE 3R	D FLOOR
BROOKLYN	СТ	06234-1804	BROOKLYN	СТ	06234-1818	HARTFORD	СТ	6106

COMMON DRIVEWAY AND MAINTENANCE AGREEMENT

WHEREAS, David P. Bell and Nancy M. Bell are the owners of two parcels or lots of land (the "Lots") situated in the Town of Brooklyn, County of Windham and State of Connecticut, shown and designated as Lots 1 and 2 on a map entitled, "SUBDIVISION MAP PREPARED FOR DAVID P. BELL AND NANCY M. BELL – PRINCE HILL ROAD - BROOKLYN, CONNECTICUT – SCALE: 1"=40' – DATE: JUNE 2020 – SHEET 2 OF 4 – PC SURVEY ASSOCIATES, LLC – 63 SNAKE MEADOW RD, KILLINGLY, CT 06239", filed in the office of the Brooklyn Town Clerk, to which map reference is hereby made for a more particular description and location of said premises; and

WHEREAS, the above Lots are to be served by a single common driveway to be located within a Right-of-Way located on Lot 1 as depicted on the above referenced map, and more particularly bounded and described as follows:

Commencing at a point in the southeasterly line of Prince Hill Road at the southwesterly corner of the herein described Right-of-Way and the northwesterly corner of other land now or formerly of David P. & Nancy M. Bell;

Thence N 38° 41' 45" E, 67.81 feet along the southeasterly line of Prince Hill Road to a point;

Thence S 63° 08' 54" E, 75.00 feet to a point;

Thence N 43° 03' 27" E, 89.23 feet to a point in the southerly line of Lot 2;

Thence S 63° 08' 54" E, 52.07 feet along the southerly line of Lot 2 to a point;

Thence S 43° 03' 27" W, 158.35 feet to a point in the northerly line of other land now or formerly of David P. & Nancy M. Bell;

Thence N 63° 08' 54" W, 121.70 feet to the point of beginning, bounded southerly by other land now or formerly of David P. & Nancy M. Bell; and

WHEREAS, said David P. Bell & Nancy M. Bell wish to set forth the respective rights and obligations of all current and future owners of said Lots;

NOW THEREFORE, it is hereby resolved that:

(1) The owners of the Lots shall share in the costs of constructing, repairing and maintaining the driveway in good condition and repair in the following percentages: Lot 1: 50% and Lot 2: 50%. Maintenance and repair shall include but not be limited to, replacement, restoration and repair of the driveway surface including but not limited to bituminous concrete, concrete, tar, asphalt, stone and gravel, snow and ice removal, removal of fallen trees and other obstructions, etc. Each Lot owner agrees to provide the owners of the other Lot with copies of bills, invoices or statements relating to the costs of repairing and maintaining the driveway.

(2) Items of repair and/or maintenance to be performed shall be determined as agreed upon by the owners of the Lots, provided however, that neither Lot owner may unreasonably withhold his or her consent of agreement to items of repair or maintenance reasonably necessary to keep the driveway in good and passable condition and repair and repair of public utilities if required.

(3) Each Lot owner shall be liable to contribute to the cost in the percentages set forth in paragraph number one (1) above, such amounts to be paid promptly upon completion of such repair or maintenance item, and after presentment of a copy of the invoice from the contractor performing the work.

(4) Notwithstanding the foregoing obligation to share equally in maintenance and repair expenses, in the event that the owner of either Lot, or such owner's agents, contractors, employees or guests shall damage the driveway by construction activities related to such owner's Lot or by negligence, reckless or intentional act, then such owner shall promptly repair such damage and restore the driveway to its prior condition, at his/her sole expense.

(5) It is mutually agreed that the owners of Lots 1 and 2 shall have the right to install underground public utilities along said driveway at their sole expense, and the owners of Lots 1 and 2 are hereby granted ingress and egress over said driveway and access is hereby granted over said driveway to any utility company (including, but not limited to electric, telephone, gas, water, sewer, cable TV) for installation / maintenance / repair of such services to any Lot. Any land disturbed by the exercise of these rights shall be restored, by the party exercising the right, to the condition it was prior to such entry.

(6) No owners of the Lots, or an owner's agent(s) or guest(s) shall in any way at any time obstruct the driveway, park vehicles or allow any to be parked on the driveway or in any other way prevent or hinder free passage by an owner, or an owner's agent(s) or guest(s) over such portion of the driveway Right-of-Way which serves each owners' property.

(7) The owners of Lots 1 and 2 and such future owners of Lots 1 and 2, covenant and agree to indemnify, defend and hold each other harmless against all claims, demands, loss, damage, liabilities and expenses and all suits, actions and judgments (including, but not limited to costs and reasonable attorneys' fees) arising out of or in any way related to their failure to maintain in a safe condition the easements granted and created hereunder. The owners of Lots 1 and 2 shall give prompt and timely notice of any claim or suit or action commenced against the other party which in any way would result in indemnification hereunder.

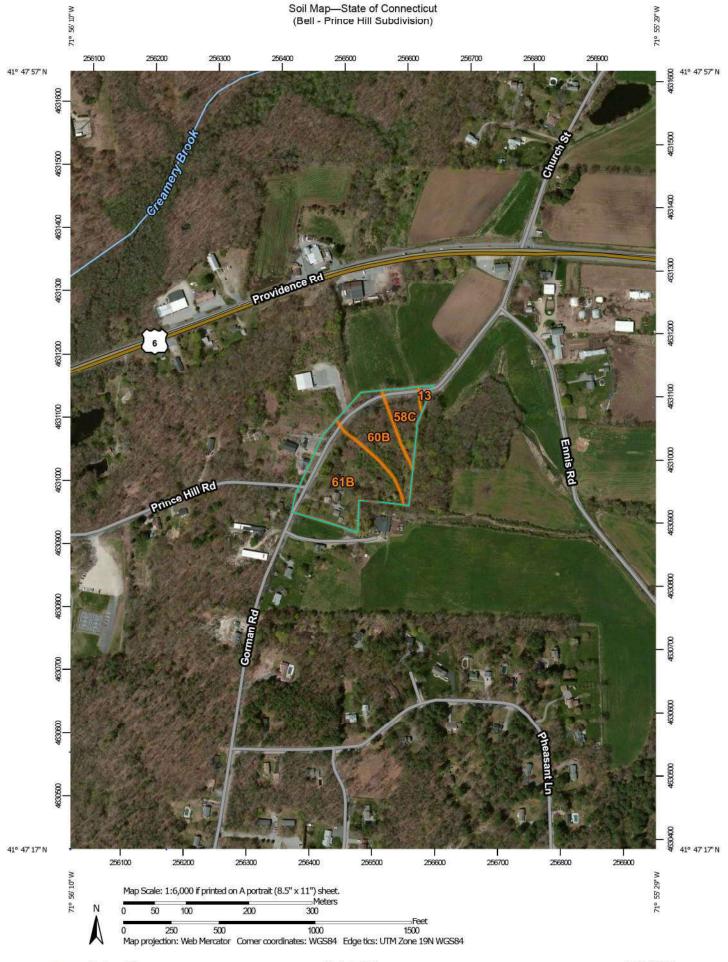
(8) In the event of failure by any owner of Lots 1 and 2 to perform, fulfill or observe any agreement herein performed, fulfilled or observed by it, which failure shall continue for thirty (30) days, or in situations involving potential danger to the health and safety of persons in, on or about Lots 1 and 2 or any portion or part thereof, in each case after written notice specifying such, the other party may, at its election, cure such failure or breach for and on behalf of the defaulting party, and any amount which the party so electing shall expend for such purpose, or which shall otherwise be due by either party to the other hereunder, shall be paid to the party to whom due upon demand, without contest upon delivery of its invoice, together with interest thereon at the lower of (i) the rate of 10% per annum, or (ii) the maximum rate permissible from time to time under applicable law, from the date of expenditure or the date when same shall have become due to the date of payment thereof in full.

(9) This agreement shall be binding on the owners of the Lots, their heirs, successors and assigns and inure to their respective benefit, shall be appurtenant to, run with, and benefit Lots 1 and 2 as shown on the aforementioned map.

(10) Any disagreement between any owner of Lot 1 and/or Lot 2 delineated on the subdivision map referred to in the first recital paragraph of this Declaration with respect to the interpretation or application of these covenants and their obligations hereunder shall be determined by arbitration. Such arbitration shall be conducted upon the request of any land owner of Lot 1 and/or Lot 2 delineated on the subdivision map referred to in the first recital paragraph of this Declaration. Any such arbitration proceeding shall be conducted in accordance with the rules of the American Arbitration Association or as may be mutually agreed. The decision of the arbitrator in any such proceeding shall be final, and judgment upon the arbitration award may be entered in any court of competent jurisdiction. The expense of arbitration proceedings conducted hereunder shall be borne

equally by the parties. Notwithstanding the foregoing, the parties agree that any and all disputes less than \$5,000.00 (or the then monetary limit of Small Claims Court) shall be heard in Small Claims Court. In any event, the successful party shall be entitled to recover all costs and expenses incurred including reasonable attorneys' fees to be fixed as a result of Arbitration or by Small Claims Court.

IN WITNESS WHEREOF, David Bell and Nancy M. Bell have hereunto set their hands and seals this day of



USDA

Web Soil Survey National Cooperative Soil Survey Soil Map—State of Connecticut (Bell - Prince Hill Subdivision)

MAP INFORMATION	The soil surveys that comprise your AOI were mapped at 1:12,000.	Please rely on the bar scale on each map sheet for map measurements.	Source of Map: Natural Resources Conservation Service	Coordinate System: Web Mercator (EPSG:3857)	Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts	distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more	accurate calculations of distance or area are required.	This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.	Soil Survey Area: State of Connecticut	Survey Area Data: Version 19, Sep 13, 2019	Soil map units are labeled (as space allows) for map scales	1.50, UUU or larger.	Date(s) aerial images were photographed: Apr 14, 2011—Aug 27, 2016	The orthophoto or other base map on which the soil lines were	compiled and digitized probably differs from the background images disclaved or these mans. As a result some minor	shifting of map unit boundaries may be evident.								
EGEND	Spoil Area	S Very Stony Spot		△ Other Special in Features	Water Features	Streams and Canals	I ransportation Rails		US Routes	Major Roads	Local Roads	Background	Aerial Photography											
MAP LE	erest (AOI) Area of Interest (AOI)	Soil Man Unit Polygons	Soil Map Unit Lines	Soil Map Unit Points	Special Point Features	Borrow Pit	Clay Spot	Closed Depression	Gravel Pit	Gravelly Spot	Landfill	Lava Flow	Marsh or swamp	Mine or Quarry	Miscellaneous Water	Perennial Water	Rock Outcrop	Saline Spot	Sandy Spot	Severely Eroded Spot	Sinkhole	Slide or Slip	Sodic Spot	
	Area of Interest (AOI) Area of Interest (AOI)	Soils	1		Special F		×	0	አ	**	0	×	-1	¢	0	0	>	+	0 0 0 0	Ŵ	0	A	R	

٦

r

Map Unit Legend

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
13	Walpole sandy loam, 0 to 3 percent slopes	0.1	1.6%
58C	Gloucester gravelly sandy loam, 8 to 15 percent slopes, very stony	1.1	14.1%
60B	Canton and Charlton fine sandy loams, 3 to 8 percent slopes	2.2	29.6%
61B	Canton and Charlton fine sandy loams, 0 to 8 percent slopes, very stony	4.1	54.6%
Totals for Area of Interest		7.5	100.0%

PER SECTION 8-26 OF THE CONNECTICUT GENERAL STATUTES, AMENDED, APPROVAL AUTOMATICALLY EXPIRES IF ALL PHYSICAL IMPROVEMENTS REQUIRED BY THIS PLAN ARE NOT COMPLETED BY THAT DATE

SHEET 1 SUBDIVISION COVER SHEET SHEET 2 SUBDIVISION BOUNDARY MAP SHEET 3 LOT DEVELOPMENT PLAN SHEET 4 EROSION & SEDIMENT CONTROL PLAN & DETAILS

APPROVED BY THE BROOKLYN INLAND WETLANDS AND WATERCOURSES COMMISSION.

THE SUBDIVISION REGULATIONS OF THE TOWN OF BROOKLYN ARE A PART

OF THIS PLAN. APPROVAL OF THIS PLAN IS CONTIGENT ON COMPLETION OF THE REQUIREMENTS OF SAID REGULATIONS, EXCEPTING ANY VARIANCE OR

MODIFICATIONS MADE BY THE COMMISSION. ANY SUCH VARIANCE OR MODIFICATIONS ARE ON FILE IN THE OFFICE OF THE COMMISSION.

APPROVED BY THE BROOKLYN PLANNING & ZONING COMMISSION

CHAIRMAN

CHAIRMAN

DATE

DATE



GRANTOR	GRANTEE	VOL.	PG.	DATE	
WILLIAM COOKE, TRUSTEE	DAVID P. & NANCY M. BELL	165	217	6/9/1988	8 ACRES, MORE OR LESS
ROBERT A. DUMAINE	WILLIAM COOKE, TRUSTEE	91	559	6/8/1988	8 ACRES, MORE OR LESS
GEORGE I. & SARA H. JOHNSON	ROBERT A. DUMAINE	91	1	4/29/1988	8 ACRES, MORE OR LESS
PAUL B. & RUTH Y. KERR	GEORGE I. & SARA H. JOHNSON	29	425	7/22/1948	8 ACRES, MORE OR LESS

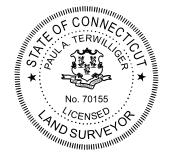
SHEET INDEX

MAP 34

LOT 2A N/F JOAN BUVARSKY

VOL. 426 , PG. 29

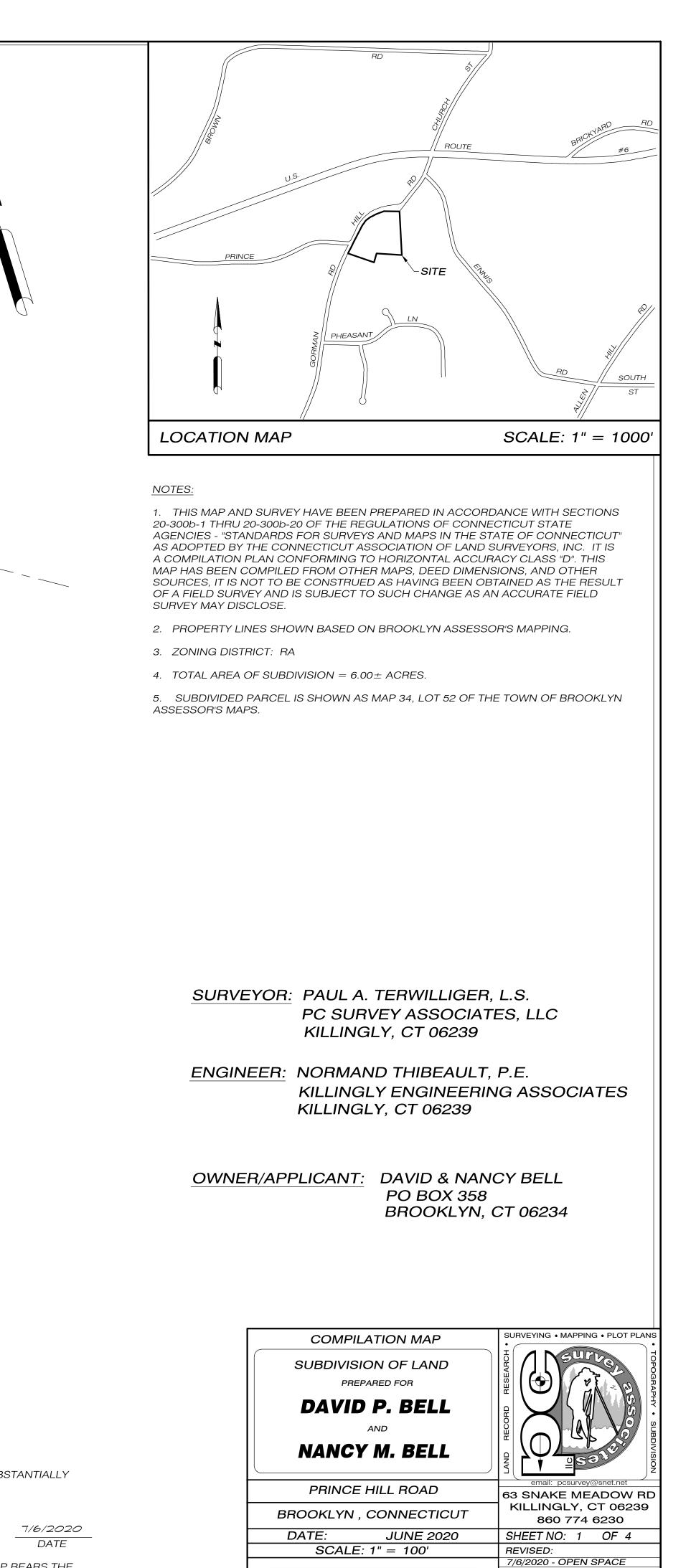
 $1.86\pm ACRES$



TO THE BEST OF MY KNOWLEDGE AND BELIEF, THIS MAP IS SUBSTANTIALLY CORRECT AS NOTED HEREON

...S. NO

NO CERTIFICATION IS EXPRESSED OR IMPLIED UNLESS THIS MAP BEARS THE EMBOSSED SEAL OF THE LAND SURVEYOR WHOSE SIGNATURE APPEARS HEREON.

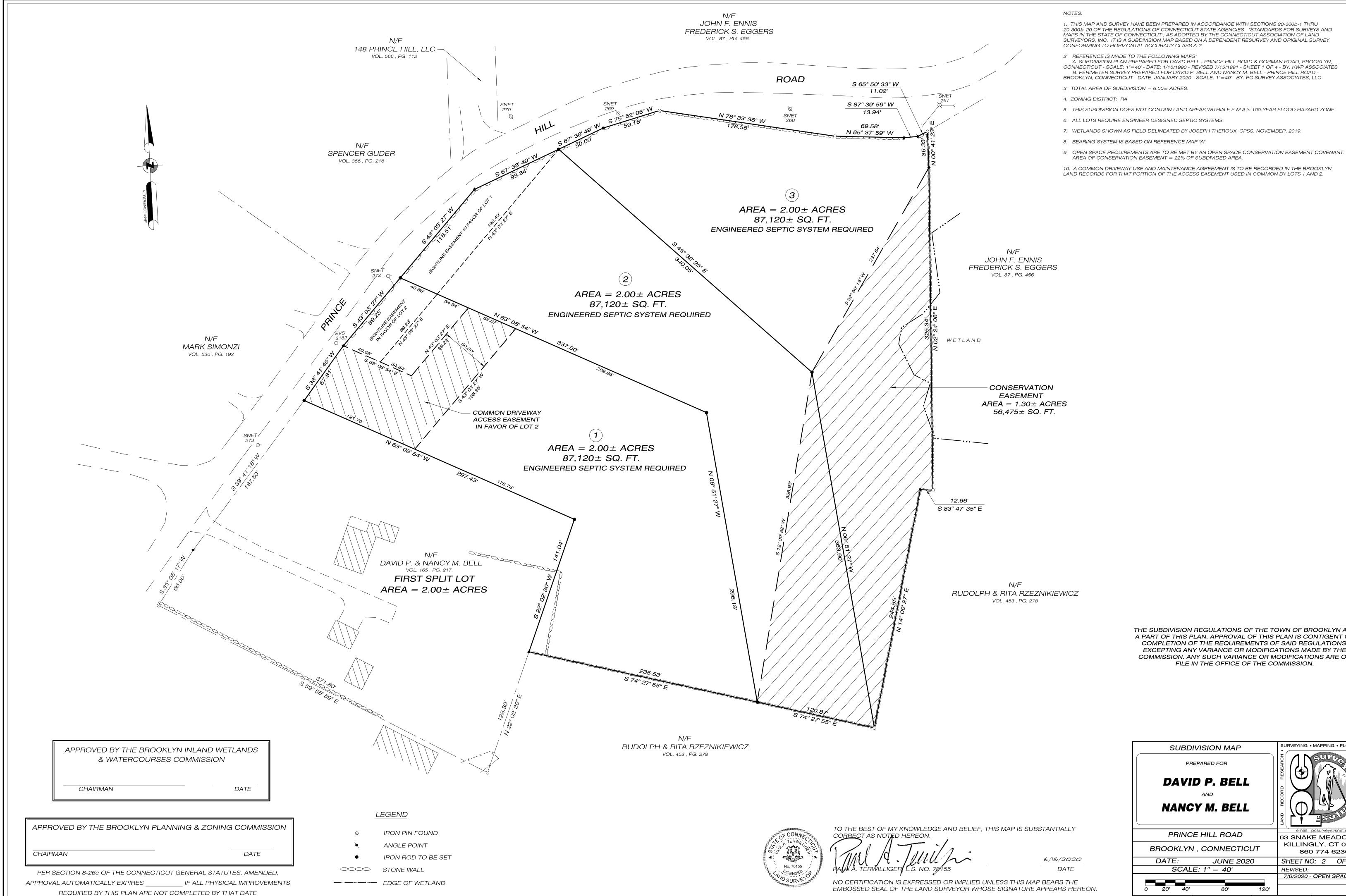


DRAWN BY: P.A.T. MAP NO: JOB NO: 18015 F.B. NO: N/A

300'

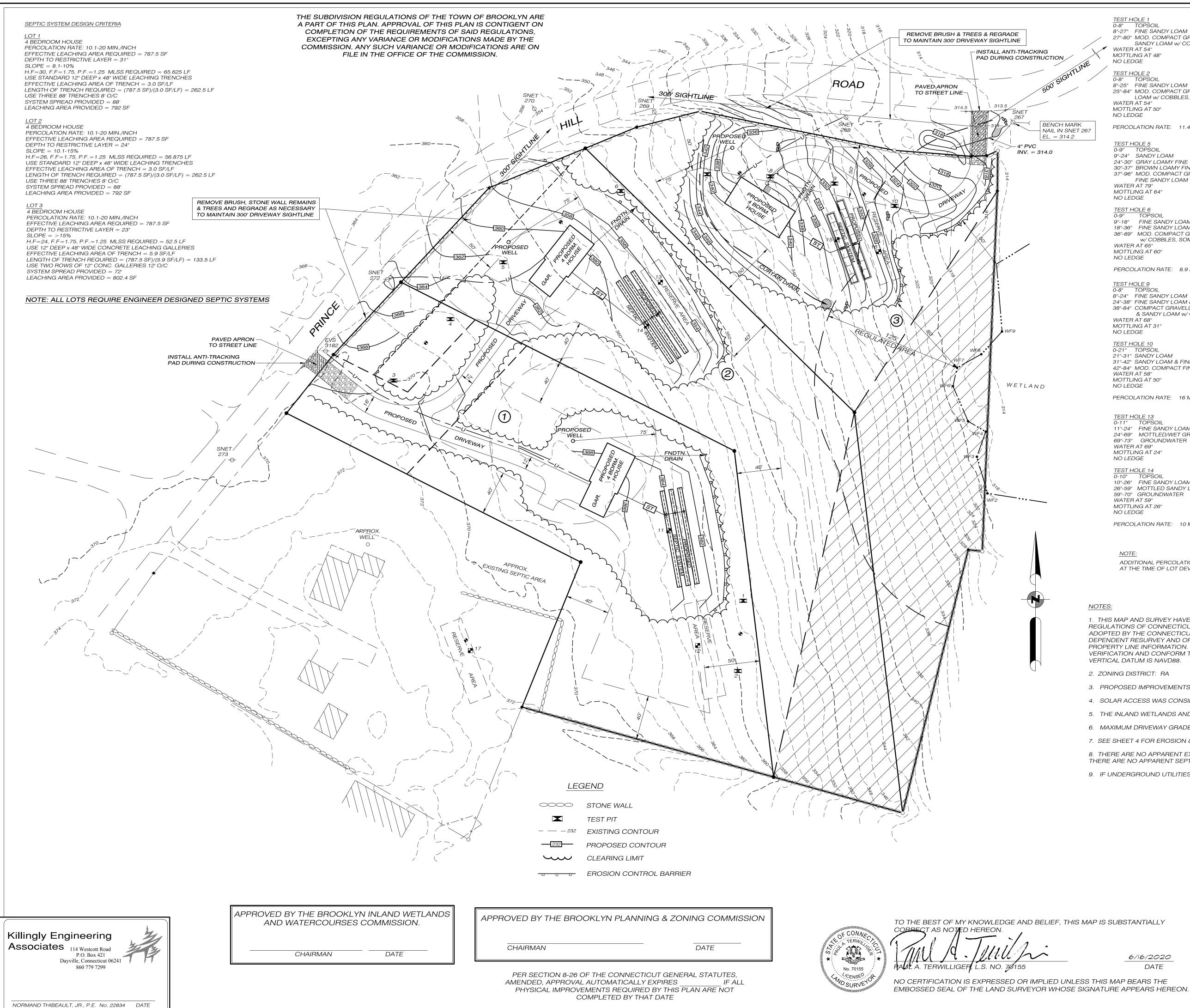
200'

50' 100'



THE SUBDIVISION REGULATIONS OF THE TOWN OF BROOKLYN ARE A PART OF THIS PLAN. APPROVAL OF THIS PLAN IS CONTIGENT ON COMPLETION OF THE REQUIREMENTS OF SAID REGULATIONS, EXCEPTING ANY VARIANCE OR MODIFICATIONS MADE BY THE COMMISSION. ANY SUCH VARIANCE OR MODIFICATIONS ARE ON

PREPARED FOR DAVID P. BELL AND NANCY M. BELL PRINCE HILL ROAD 63 SNAKE MEADOW RD KILLINGLY, CT 06239 BROOKLYN, CONNECTICUT BROOKLYN, CONNECTICUT		SUBD	IVISION MAP		SURVEYING	MAPPING • F	PLOT PLA	NS
ELIEF, THIS MAP IS SUBSTANTIALLY PRINCE HILL ROAD G3 SNAKE MEADOW RD 6/16/2020 DATE JUNE 2020 SHEET NO: 2 OF 4 DATE SCALE: 1" = 40' REVISED: 7/6/2020 - OPEN SPACE		PF	REPARED FOR		SEARCH	SULLA	2/1	TOPOGI
ELIEF, THIS MAP IS SUBSTANTIALLY NANCY M. BELL Image: posurvey@snet.net PRINCE HILL ROAD 63 SNAKE MEADOW RD BROOKLYN , CONNECTICUT 63 SNAKE MEADOW RD BROOKLYN , CONNECTICUT 860 774 6230 DATE DATE: JUNE 2020 LIED UNLESS THIS MAP BEARS THE 0 20' 40' 80' 120'		DAVI				K	SS SS SS	RAPHY •
ELIEF, THIS MAP IS SUBSTANTIALLY PRINCE HILL ROAD email: pcsurvey@snet.net PRINCE HILL ROAD 63 SNAKE MEADOW RD BROOKLYN , CONNECTICUT 63 SNAKE MEADOW RD BROOKLYN , CONNECTICUT 860 774 6230 DATE DATE: JUNE 2020 DATE SCALE: 1" = 40' REVISED: NHOSE SIGNATURE APPEARS HEREON. 0 20' 40' 80' 120' 7/6/2020 - OPEN SPACE		NANC				<u>0</u> 50 ³	V 0 9)*	SUBDIVISIO
PRINCE HILL ROAD63 SNAKE MEADOW RD KILLINGLY, CT 06239 860 774 62306/16/2020BROOKLYN , CONNECTICUT63 SNAKE MEADOW RD KILLINGLY, CT 06239 860 774 6230DATEJUNE 2020SHEET NO: 2 OF 4DATESCALE: 1" = 40'REVISED:LIED UNLESS THIS MAP BEARS THE R WHOSE SIGNATURE APPEARS HEREON.0 20' 40' 80' 120'	THE THIS MAP IS SUBSTANTIALLY						t not	Ĵž
6/16/2020 BROOKLYN, CONNECTICUT 860 774 6230 DATE DATE: JUNE 2020 SHEET NO: 2 OF 4 LIED UNLESS THIS MAP BEARS THE SCALE: 1" = 40' REVISED: R WHOSE SIGNATURE APPEARS HEREON. 0 20' 40' 80' 120' 7/6/2020 - OPEN SPACE		PRINC	E HILL ROAD					D
DATE DATE DATE DATE DATE DATE DATE DATE	*	BROOKLYN	N, CONNECTIC	JT)
LIED UNLESS THIS MAP BEARS THE R WHOSE SIGNATURE APPEARS HEREON. 7/6/2020 - OPEN SPACE 7/6/2020 - OPEN SPACE 120'	6/16/2020	DATE:	JUNE 202	0	SHEET N	D: 2 O	F 4	
LIED UNLESS THIS MAP BEARS THE 0 20' 40' 80' 120'	DATE	SCAL	.E: 1" = 40'					
JOB NO: 18015 F.B. NO: N/A DRAWN BY: P.A.T. MAP NO:	LIED UNLESS THIS MAP BEARS THE R WHOSE SIGNATURE APPEARS HEREON.	0 20' 40'	80'	120'	7/6/2020 -	OPEN SPA	ACE	
		JOB NO: 18015	F.B. NO: N/A	DRAWN	BY: P.A.T.	MAP NO:		



TEST HOLE 1 0-8" TOPSOIL

8"-27" FINE SANDY LOAM 27"-80" MOD. COMPACT GRAVELLY LOAMY SAND, SANDY LOAM w/ COBBLES 💉 WATER AT 54" MOTTLING AT 48"

TEST HOLE 2 0-8" TOPSOIL

NO LEDGE

8"-25" FINE SANDY LOAM 25"-84" MOD. COMPACT GRAVELLY LOAMY SAND & SANDY LOAM w/ COBBLES, SOME STONES WATER AT 54" MOTTLING AT 50" NO LEDGE

PERCOLATION RATE: 11.4 MIN./INCH

TEST HOLE 5 0-9" TOPSOIL

9"-24" SANDY LOAM 24"-30" GRAY LOAMY FINE SAND 30"-37" BROWN LOAMY FINE/MED. SAND 37"-96" MOD. COMPACT GRAVELLY LOAMY FINE SAND & FINE SANDY LOAM w/ COBBLES WATER AT 79" MOTTLING AT 64"

NO LEDGE TEST HOLE 6 0-9" TOPSOIL 9"-18" FINE SANDY LOAM 18"-36" FINE SANDY LOAM & SANDY LOAM 36"-89" MOD. COMPACT GRAVELLY LOAMY SAND & SANDY LOAM w/ COBBLES, SOME FLAT ROCK WATER AT 65"

MOTTLING AT 60" NO LEDGE PERCOLATION RATE: 8.9 MIN./INCH

TEST HOLE 9 0-8" TOPSOIL 8"-24" FINE SANDY LOAM 24"-38" FINE SANDY LOAM & LOAMY FINE SAND 38"-84" COMPACT GRAVELLY LOAMY FINE SAND & SANDY LOAM w/ COBBLES WATER AT 68" MOTTLING AT 31" NO LEDGE TEST HOLE 10 0-21" TOPSOIL 21"-31" SANDY LOAM 31"-42" SANDY LOAM & FINE SANDY LOAM

42"-84" MOD. COMPACT FINE SANDY LOAM, SOME LOAMY SAND WATER AT 58" MOTTLING AT 50" NO LEDGE PERCOLATION RATE: 16 MIN./INCH

TEST HOLE 13 0-11" TOPSOIL 11"-24" FINE SANDY LOAM 24"-69" MOTTLED/WET GRAVELLY SANDY LOAM 69"-73" GROUNDWATER WATER AT 69" MOTTLING AT 24" NO LEDGE

TEST HOLE 14 0-10" TOPSOIL 10"-26" FINE SANDY LOAM 26"-59" MOTTLED SANDY LOAM w/ GRAVEL 59"-70" GROUNDWATER WATER AT 59" MOTTLING AT 26" NO LEDGE

PERCOLATION RATE: 10 MIN./INCH

TEST HOLE 3 0-9" TOPSOIL 9"-23" SANDY LOAM 23"-42" FINE SANDY LOAM & SANDY LOAM, SOME FINE/MED. SAND WATER AT 67" MOTTLING AT 62" NO LEDGE TEST HOLE 4 0-9" TOPSOIL 9"-21" SANDY LOAM 21"-36" LOAMY SAND 36"-100" MOD. COMPACT GRAVELLY LOAMY SAND & SANDY LOAM w/ COBBLES, POCKET LOAMY VERY FINE SAND WATER AT 60" MOTTLING AT 40" NO LEDGE

PERCOLATION RATE: 11.4 MIN./INCH

TEST HOLE 7 0-8" TOPSOIL 8"-22" FINE SANDY LOAM 22"-66" MOD. COMPACT GRAVELLY FINE SANDY LOAM, SOME LOAMY FINE SAND w/ COBBLES WATER AT 24" MOTTLING AT 23" NO LEDGE

TEST HOLE 8 0-8" TOPSOIL 8"-24" SANDY LOAM & FINE SANDY LOAM 24"-96" MOD. COMPACT GRAVELLY LOAMY FINE SAND, SOME FINE SANDY LOAM, SOME FINE/MED. SAND w/ COBBLES NO WATER NO MOTTLING

PERCOLATION RATE: 16-20 MIN./INCH

NO LEDGE

TEST HOLE 11 0-12" TOPSOIL 12"-32" FINE SANDY LOAM 32"-80" MOTTLED GRAVELLY SANDY LOAM 80-84" GROUNDWATER WATER AT 80" MOTTLING AT 32" NO LEDGE

TEST HOLE 12 0-12" TOPSOIL 12"-31" FINE SANDY LOAM 31"-76" MOTTLED GRAVELLY SANDY LOAM 76-80" GROUNDWATER WATER AT 76" MOTTLING AT 31" NO LEDGE

<u>TEST HOLE 15</u> 0-9" TOPSOIL 9"-23" FINE SANDY LOAM 23"-81" MOTTLED GRAVELLY LOAMY SAND NO WATER MOTTLING AT 23" NO LEDGE

TEST HOLE 16 0-12" TOPSOIL 12"-28" FINE SANDY LOAM 28"-70" MOTTLED GRAVELLY LOAMY SAND NO WATER MOTTLING AT 28" NO LEDGE

TEST HOLE 17 0-9" TOPSOIL 9"-28" FINE SANDY LOAM 28"-70" MOTTLED FINE SANDY LOAM w/ GRAVEL 70"-78" GROUNDWATER WATER AT 70" MOTTLING AT 28" NO LEDGE

PERCOLATION RATE: 10 MIN./INCH

ADDITIONAL PERCOLATION TESTS AT DEPTHS ABOVE RESTRICTIVE LAYER TO BE PERFORMED ON LOTS 1 AND 3 AT THE TIME OF LOT DEVELOPMENT.

NOTES:

1. THIS MAP AND SURVEY HAVE BEEN PREPARED IN ACCORDANCE WITH SECTIONS 20-300b-1 THRU 20-300b-20 OF THE REGULATIONS OF CONNECTICUT STATE AGENCIES - "STANDARDS FOR SURVEYS AND MAPS IN THE STATE OF CONNECTICUT", AS ADOPTED BY THE CONNECTICUT ASSOCIATION OF LAND SURVEYORS, INC. IT IS A GENERAL LOCATION SURVEY BASED ON A DEPENDENT RESURVEY AND ORIGINAL SURVEY CONFORMING TO HORIZONTAL ACCURACY CLASS B. SEE SHEET 2 FOR PROPERTY LINE INFORMATION. TOPOGRAPHIC FEATURES DEPICTED HEREON WERE TAKEN FROM NOAA LIDAR DATA WITH FIELD VERIFICATION AND CONFORM TO TOPOGRAPHIC ACCURACY CLASS T-3. VERTICAL DATUM IS NAVD88.

2. ZONING DISTRICT: RA

NOTE:

3. PROPOSED IMPROVEMENTS ARE CONCEPTUAL LOCATIONS TO SHOW LOT SUITABILITY ONLY.

- 4. SOLAR ACCESS WAS CONSIDERED IN THE DESIGN OF THIS SUBDIVISION.
- 5. THE INLAND WETLANDS AND WATERCOURSES WERE FIELD DELINEATED IN NOVEMBER 2019 BY JOSEPH R. THEROUX, CPSS.
- 6. MAXIMUM DRIVEWAY GRADE PERMITTED IS 12%. GRADES OF 10% OR GREATER ARE TO BE PAVED.
- 7. SEE SHEET 4 FOR EROSION & SEDIMENT CONTROL PLAN.

8. THERE ARE NO APPARENT EXISTING OR PROPOSED WELLS WITHIN 75 FEET OF THE PROPOSED SEPTIC AREAS DEPICTED. THERE ARE NO APPARENT SEPTIC AREAS WITHIN 75 FEET OF THE PROPOSED WELL LOCATIONS DEPICTED.

9. IF UNDERGROUND UTILITIES ARE INSTALLED, NO TRENCH SHALL BE LOCATED WITHIN 25' OF A SEPTIC SYSTEM OR WELL.

SURVEYING • MAPPING • PLOT PLAN\$ **GENERAL LOCATION SURVEY** LOT DEVELOPMENT PLAN SUBDIVISION OF LAND PREPARED FOR DAVID P. BELL AND NANCY M. BELL PRINCE HILL ROAD 63 SNAKE MEADOW RD KILLINGLY, CT 06239 BROOKLYN, CONNECTICUT 860 774 6230 6/16/2020 DATE: JUNE 2020 SHEET NO: 3 OF 4 DATE SCALE: 1" = 40' REVISED: 7/6/2020 - OPEN SPACE 120 20'

> JOB NO: 18015 F.B. NO: N/A DRAWN BY: P.A.T. MAP NO:

REFERENCE IS MADE TO:

- 1. CONNECTICUT GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL, MAY 2002.
- 2. SOIL SURVEY OF WINDHAM COUNTY CONNECTICUT, U.S.D.A. SOIL CONSERVATION SERVICE 1983.

DEVELOPMENT

PROPOSED DEVELOPMENT WILL CREATE THREE 2 ACRE BUILDING LOTS.

CONSTRUCTION SEQUENCE:

- 1. INSTALL EROSION AND SEDIMENT CONTROL STRUCTURES ALONG THE PROPOSED LIMITS OF DISTURBANCE.
- 2. PROVIDE ANTI TRACKING PAD AND TEMPORARY POWER TO THE SITE.
- 3. REMOVE AND STOCKPILE TOPSOIL AND INSTALL SEDIMENT BARRIER.
- 4. EXCAVATE FOUNDATION AND BEGIN HOUSE CONSTRUCTION.
- 5. INSTALL SEPTIC SYSTEM AND WELL.
- 6. INSTALL DRIVEWAY AND UTILITIES TO THE RESIDENCE.
- 7. LOAM, SEED & MULCH DISTURBED AREAS.
- 8. REMOVE EROSION AND SEDIMENT CONTROL

GENERAL DEVELOPMENT PLAN

PRIOR TO THE COMMENCEMENT OF OPERATIONS IN ACCORDANCE WITH ANY PERMIT ISSUED BY THE TOWN OF BROOKLYN PLANNING AND ZONING COMMISSION, THE CONTRACTOR SHALL INSTALL ALL EROSION AND SEDIMENT CONTROL DEVICES.

THE CONTRACTOR SHALL OBTAIN A SITE INSPECTION FROM THE TOWN OF BROOKLYN ZONING ENFORCEMENT OFFICER OR WETLANDS AGENT TO ENSURE THAT ALL EROSION AND SEDIMENT CONTROL MEASURES HAVE BEEN INSTALLED IN ACCORDANCE WITH THIS NARRATIVE. UPON APPROVAL WITH RESPECT TO THE INSTALLATION OF EROSION AND SEDIMENT CONTROL MEASURES, THE CONTRACTOR MAY COMMENCE OPERATIONS PURSUANT TO THE PERMIT. EROSION AND SEDIMENT CONTROL DEVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE "SILT FENCE INSTALLATION & MAINTENANCE" AND "HAY BALE INSTALLATION & MAINTENANCE" SECTIONS OF THIS NARRATIVE.

ALL STRIPPING IS TO BE CONFINED TO THE IMMEDIATE CONSTRUCTION AREA. TOPSOIL SHALL BE STOCKPILED SO THAT SLOPES DO NOT EXCEED 2 TO 1. THERE SHALL BE NO BURIAL OF STUMPS. A HAY BALE SEDIMENT BARRIER IS TO SURROUND EACH STOCKPILE AND A TEMPORARY VEGETATIVE COVER PROVIDED IF NECESSARY.

DUST CONTROL WILL BE ACCOMPLISHED BY SPRAYING WITH WATER.

FINAL STABILIZATION OF THE SITE IS TO FOLLOW THE PROCEDURES OUTLINED IN PERMANENT VEGETATIVE COVER. IF NECESSARY A TEMPORARY VEGETATIVE COVER IS TO BE PROVIDED UNTIL A PERMANENT COVER CAN BE APPLIED.

DURING THE STABILIZATION PERIOD, ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED IN PROPER WORKING ORDER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CHECKING ALL EROSION AND SEDIMENT CONTROL ON A TWICE-WEEKLY BASIS DURING THE STABILIZATION PERIOD AND AFTER EACH STORM EVENT. DURING THE STABILIZATION PERIOD WITH RESPECT TO THE SITE, ANY EROSION WHICH OCCURS WITHIN DISTURBED AREAS SHALL BE IMMEDIATELY REPAIRED. RESEEDED AND RE-ESTABLISHED.

ALL DISTURBED SLOPES SHALL BE STABILIZED WITHIN ONE SEASON (SPRING OR FALL) OF THE COMPLETION OF THE PROJECT BEFORE A CERTIFICATE OF COMPLIANCE WILL BE ISSUED

ONCE STABILIZATION HAS BEEN COMPLETED AND APPROVED BY THE TOWN OF BROOKLYN ZONING ENFORCEMENT OFFICER, EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED BY THE CONTRACTOR.

SILT FENCE INSTALLATION AND MAINTENANCE:

1. DIG A 6" DEEP TRENCH ON THE UPHILL SIDE OF THE BARRIER LOCATION.

2. POSITION THE POSTS ON THE DOWNHILL SIDE OF THE BARRIER AND DRIVE THE POSTS 1.5 FEET INTO THE GROUND.

3. LAY THE BOTTOM 6" OF THE FABRIC IN THE TRENCH TO PREVENT UNDERMINING AND BACKFILL.

4. INSPECT AND REPAIR BARRIER AFTER HEAVY RAINFALL.

5. INSPECTIONS WILL BE MADE AT LEAST ONCE PER WEEK AND WITHIN 24 HOURS OF THE END OF A STORM WITH A RAINFALL AMOUNT OF 0.5 INCH OR GREATER TO DETERMINE MAINTENANCE NEEDS.

6. SEDIMENT DEPOSITS ARE TO BE REMOVED WHEN THEY REACH A HEIGHT OF 1 FOOT BEHIND THE BARRIER OR HALF THE HEIGHT OF THE BARRIER AND ARE TO BE DEPOSITED IN AN AREA WHICH IS NOT REGULATED BY THE INLAND WETLANDS COMMISSION.

7. REPLACE OR REPAIR THE FENCE WITHIN 24 HOURS OF OBSERVED FAILURE. FAILURE OF THE FENCE HAS OCCURRED WHEN SEDIMENT FAILS TO BE RETAINED BY THE FENCE BECAUSE:

- THE FENCE HAS BEEN OVERTOPPED, UNDERCUT OR BYPASSED BY RUNOFF WATER, - THE FENCE HAS BEEN MOVED OUT OF POSITION, OR

- THE GEOTEXTILE HAS DECOMPOSED OR BEEN DAMAGED.

HAY BALE INSTALLATION AND MAINTENANCE:

1. BALES SHALL BE PLACED AS SHOWN ON THE PLANS WITH THE ENDS OF THE BALES TIGHTLY ABUTTING EACH OTHER.

2. EACH BALE SHALL BE SECURELY ANCHORED WITH AT LEAST 2 STAKES AND GAPS BETWEEN BALES SHALL BE WEDGED WITH STRAW TO PREVENT WATER FROM PASSING BETWEEN THE BALES.

3. INSPECT BALES AT LEAST ONCE PER WEEK AND WITHIN 24 HOURS OF THE END OF A STORM WITH A RAINFALL AMOUNT OF 0.5 INCHES OR GREATER TO DETERMINE MAINTENANCE NEEDS.

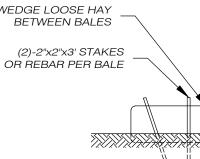
4. REMOVE SEDIMENT BEHIND THE BALES WHEN IT REACHES HALF THE HEIGHT OF THE BALE AND DEPOSIT IN AN AREA WHICH IS NOT REGULATED BY THE INLAND WETLANDS COMMISSION.

5. REPLACE OR REPAIR THE BARRIER WITHIN 24 HOURS OF OBSERVED FAILURE. FAILURE OF THE BARRIER HAS OCCURRED WHEN SEDIMENT FAILS TO BE RETAINED BY THE BARRIER BECAUSE:

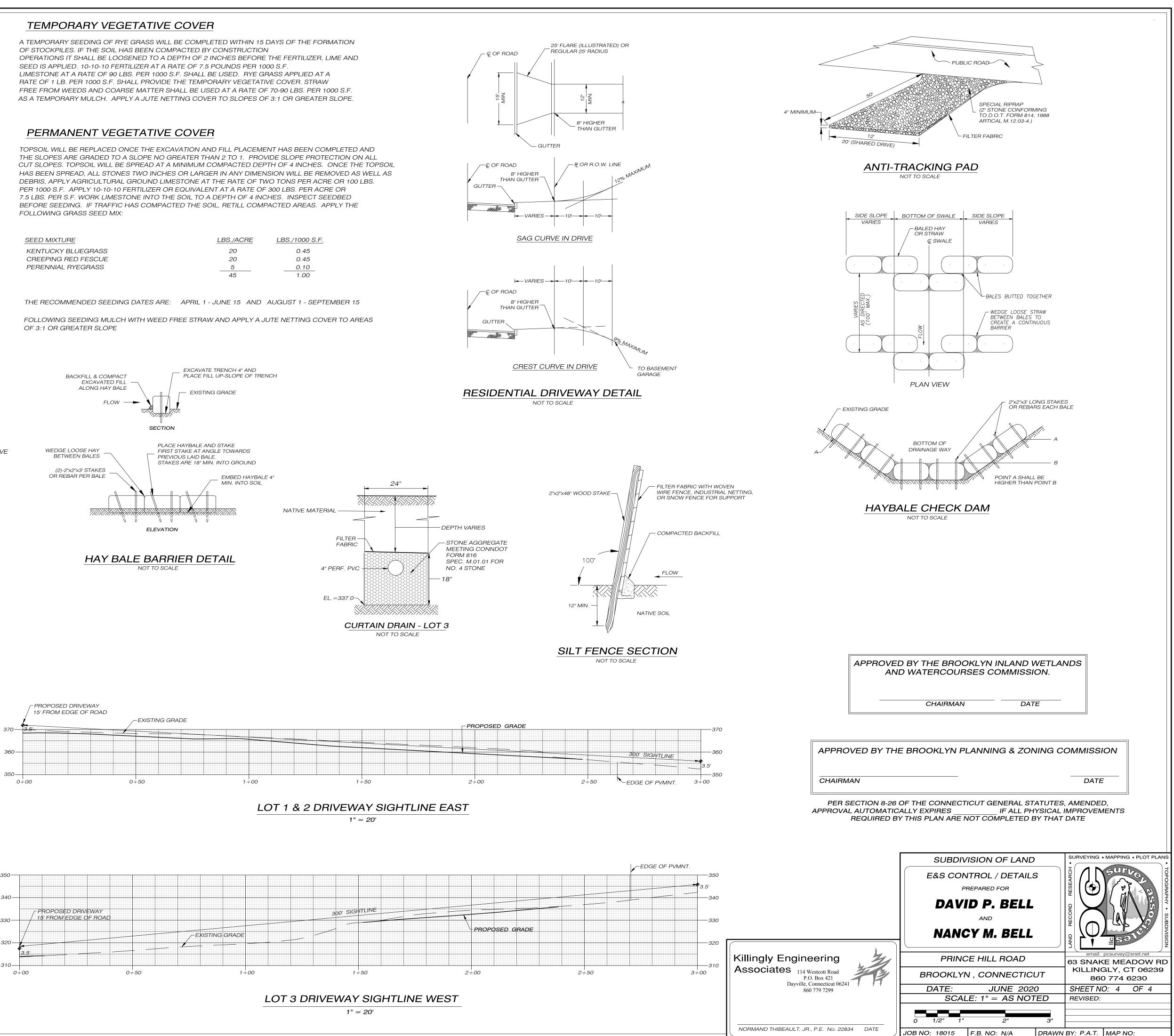
- THE BARRIER HAS BEEN OVERTOPPED. UNDERCUT OR BYPASSED BY RUNOFF WATER. - THE BARRIER HAS BEEN MOVED OUT OF POSITION. OR

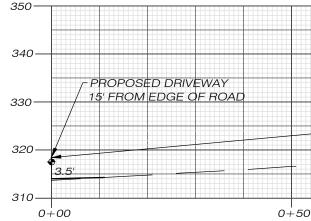
- THE HAY BALES HAVE DETERIORATED OR BEEN DAMAGED.

BACKFILL & COMPACT EXCAVATED FILL ALONG HAY BALE FLOW

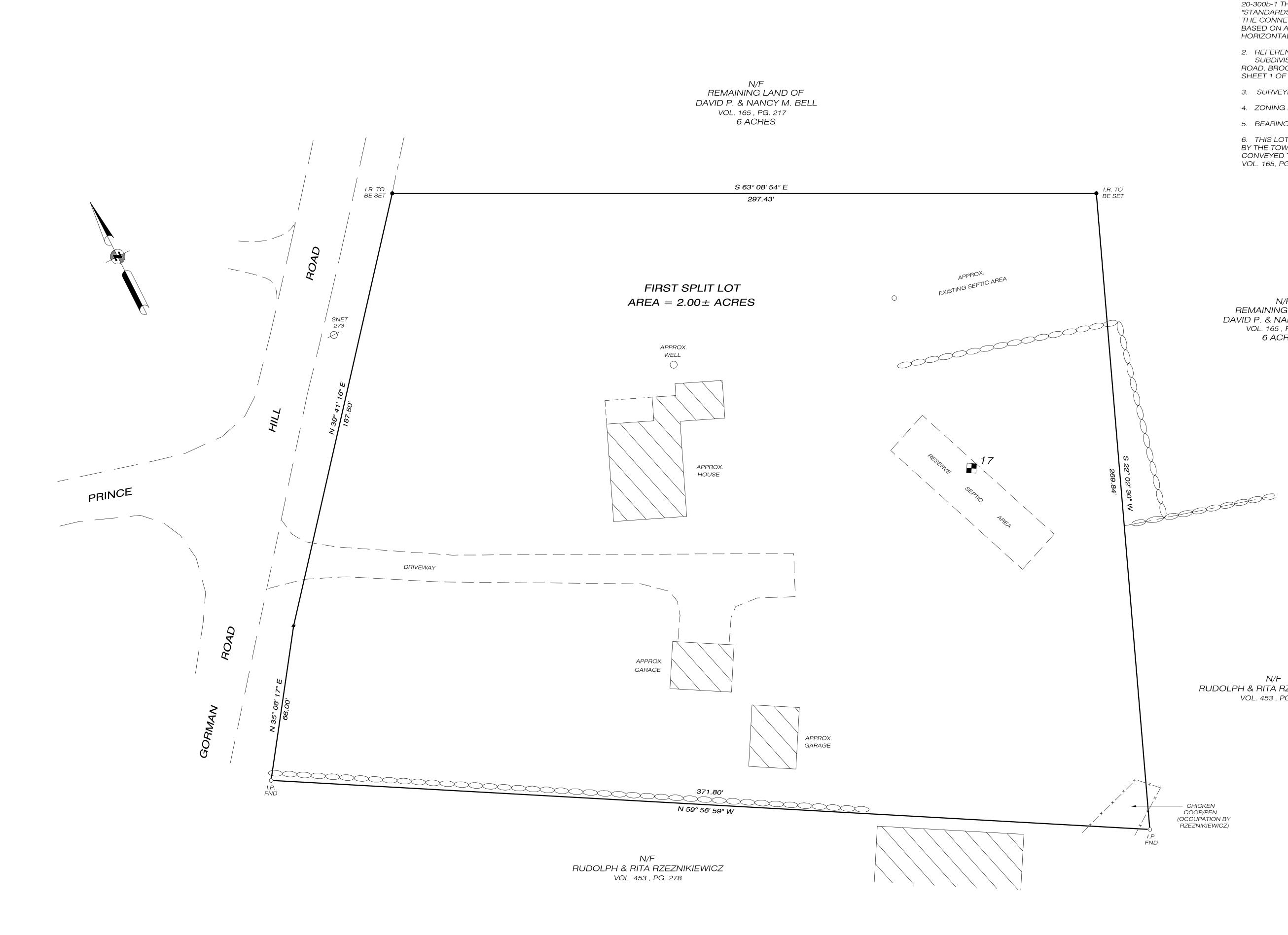












PARCEL HISTORY

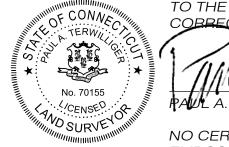
GRANTOR WILLIAM COOKE, TRUSTEE ROBERT A. DUMAINE GEORGE I. & SARA H. JOHNSON R PAUL B. & RUTH Y. KERR

GRANTEE	VOL
DAVID P. & NANCY M. BELL	165
WILLIAM COOKE, TRUSTEE	91
ROBERT A. DUMAINE	91
GEORGE I. & SARA H. JOHNSON	29

_ ·	PG.	
	217	
	559	
	1	
	425	

DATE 6/9/1988 6/8/1988

8 ACRES, MORE OR LESS 8 ACRES, MORE OR LESS 4/29/1988 8 ACRES, MORE OR LESS 7/22/1948 8 ACRES, MORE OR LESS



TED HEREON A. TERWILLIGER, L.S. NO. 70155

NO CERTIFICATION IS EXPRESSED OR IMPLIED U EMBOSSED SEAL OF THE LAND SURVEYOR WHOSE SIGNATURE APPEARS HEREON.

NOTES:

1. THIS MAP AND SURVEY HAVE BEEN PREPARED IN ACCORDANCE WITH SECTIONS 20-300b-1 THRU 20-300b-20 OF THE REGULATIONS OF CONNECTICUT STATE AGENCIES -"STANDARDS FOR SURVEYS AND MAPS IN THE STATE OF CONNECTICUT", AS ADOPTED BY THE CONNECTICUT ASSOCIATION OF LAND SURVEYORS, INC. IT IS A PERIMETER SURVEY BASED ON A DEPENDENT RESURVEY AND ORIGINAL SURVEY CONFORMING TO HORIZONTAL ACCURACY CLASS A-2.

2. REFERENCE IS MADE TO THE FOLLOWING MAP:

SUBDIVISION PLAN PREPARED FOR DAVID BELL - PRINCE HILL ROAD & GORMAN ROAD, BROOKLYN, CONNECTICUT - SCALE: 1"=40' - DATE: 1/15/1990 - REVISED 7/15/1991 -SHEET 1 OF 4 - BY: KWP ASSOCIATES

3. SURVEYED PARCEL IS A PORTION OF BROOKLYN ASSESSOR'S MAP #34 , LOT #52.

- 4. ZONING DISTRICT: RA
- 5. BEARING SYSTEM IS BASED ON REFERENCE MAP.

6. THIS LOT IS THE FIRST LOT CUT SINCE THE ADOPTION OF SUBDIVISION REGULATIONS BY THE TOWN OF BROOKLYN FROM A PARCEL OF LAND CONTAINING 8 ACRES AS CONVEYED TO DAVID P. & NANCY M. BELL BY DEED DATED JUNE 9, 1988, RECORDED IN VOL. 165, PG. 217 OF THE BROOKLYN LAND RECORDS.

N/F REMAINING LAND OF DAVID P. & NANCY M. BELL VOL. 165 , PG. 217 6 ACRES

SOIL TESTING PERFORMED 1/16/2020 BY NDDH

TEST HOLE 17 0-8" TOPSOIL 8"-27" FINE SANDY LOAM 27"-80" MOD. COMPACT GRAVELLY LOAMY SAND, SANDY LOAM w/ COBBLES WATER AT 54" MOTTLING AT 48"

PERCOLATION RATE: 11.4 MIN./INCH

NO LEDGE

N/F RUDOLPH & RITA RZEZNIKIEWICZ VOL. 453 , PG. 278

– CHICKEN COOP/PEN (OCCUPATION BY RZEZNIKIEWICZ)

IP FND

PERIMETER SURVEY	SURVEYING • MAPPING • PLOT PLANS
PREPARED FOR	HESEARCH
DAVID P. BELL	
NANCY M. BELL	IAND RECORD
PRINCE HILL ROAD	email: pcsurvey@snet.net 63 SNAKE MEADOW RD
BROOKLYN , CONNECTICUT	KILLINGLY, CT 06239 860 774 6230
DATE: JANUARY 2020	SHEET NO: 1 OF 1
SCALE: 1" = 40'	REVISED:
0 20' 40' 80' 120'	
JOB NO: 18015 F.B. NO: N/A DRAWN	BY: P.A.T. MAP NO:

TO THE BEST OF MY KNOWLEDGE AND BELIEF, THIS MAP IS SUBSTANTIALLY

DA	ATE
UNLESS THIS MAP BEARS	THE

1/30/2020

PERMANENT CONSERVATION EASEMENT COVENANT

WHEREAS, David P. Bell and Nancy M. Bell are the owners of two parcels or lots of land (the "Lots") situated in the Town of Brooklyn, County of Windham and State of Connecticut, shown and designated as Lots 2 and 3 on a map entitled, "SUBDIVISION MAP PREPARED FOR DAVID P. BELL AND NANCY M. BELL – PRINCE HILL ROAD, BROOKLYN, CONNECTICUT – SCALE: 1"=40' – DATE: JUNE 2020 – REVISED 7/6/2020 – SHEET 2 OF 4 - PC SURVEY ASSOCIATES, LLC – 63 SNAKE MEADOW RD, KILLINGLY, CT 06239", as filed in the office of the Brooklyn Town Clerk, to which map reference is hereby made for a more particular description and location of said premises; and

WHEREAS, the above Lots are to be SUBJECT TO a Conservation Easement as depicted on the above referenced map, and more particularly bounded and described as follows:

A certain Conservation Easement over Lot 2 and Lot 3 bounded and described as follows: Beginning at a point in a stone wall in the easterly line of Lot 3 and the westerly line of land now or formerly of John F. Ennis and Frederick S. Eggers, being S 00° 41' 25" E, 36.33 feet from a T-Bar at a wall corner in the southerly line of Prince Hill Road at the northeasterly corner of said Lot 3 and the northwesterly corner of land of said Ennis and Eggers; Thence S 32° 50' 14" W, 237.64 feet to a point at an angle in the northeasterly line of Lot 2; Thence S 12° 30' 52" W, 336.93 feet to a point in a stone wall in the northerly line of land now or formerly Rudolph & Rita Rzeznikiewicz at the southeasterly corner of Lot 1; Thence S 74° 27' 55" E, 120.87 feet along a stone wall to a T-Bar in a wall corner; Thence S 83° 47' 35" E, 12.66 feet to a T-bar in a wall corner, the last three courses bounded southerly and easterly by land now or formerly Rudolph & Rita Rzeznikiewicz; Thence N 02° 24' 08" E, 325.34 feet along a stone wall to the point of beginning, bounded easterly by land now or formerly of John F. Ennis and Frederick S. Eggers; and

WHEREAS, said David P. Bell and Nancy M. Bell wish to set forth the respective rights, restrictions and obligations of all current and future owners of said Lots;

NOW THEREFORE, it is hereby resolved that:

Lots 2 and 3 shall be conveyed subject to said Conservation Easements and restrictions and covenants set forth in the various paragraphs of this document.

This Conservation Easement ("Easement") shall be deemed to be a covenant that runs with the land and shall be binding upon David P. & Nancy M. Bell, their successors and assigns and all persons claiming through the Grantor in perpetuity.

If the Grantor, its successors and assigns, or any person claiming under them shall violate or attempt to violate any of the covenants herein, it shall be lawful for the Town of Brooklyn, through its Planning and Zoning Commission, to take any and all steps necessary to enforce this Conservation Easement, to prosecute any proceedings at law or in equity against the person or persons violating or attempting to violate any such covenant and either to prevent him or them from so doing and to recover damages incurred, to recover all attorney's fees and costs or any other expenses incurred in enforcing this Conservation Easement or in correcting the results of any violation of said Easement.

The Grantor shall include in the conveyance of Lots 2 & 3 the right to enforce this Open Space Easement against any party in violation of any provision of this Conservation Easement. Said conveyance shall contain the language:

"Said premises are conveyed together with the right to enforce the Conservation Easement for purposes of preserving and protecting Open Space that is conveyed as a part of said property, all as provided in said Conservation Easement as of record appears."

To preserve and protect the Easement in perpetuity, the following restrictions on use and activities shall be enforced in perpetuity with respect to said Easement:

- 1. No structure of any kind may be built on said Easement.
- 2. No motorized vehicles shall enter upon or travel across the Easement.
- 3. No dumping shall occur on said Easement.
- 4. No fires shall occur on said Easement.
- 5. No mining or natural resource extraction shall occur on said Easement.
- 6. No harvesting of timber or firewood except as part of a long term management plan prepared by a professional forester and approved in advance by the Brooklyn Conservation Commission shall occur in the easement. No liquidation cuts or clearcutting are allowed on said Easement.
- 7. The Lot owners shall have the right to use the easement areas located on their respective Lots for passive recreational activities.

Invalidation of any of these covenants by judgment or court order shall in no way affect any of the other provisions which shall remain in full force and effect.

TO HAVE AND TO HOLD the above granted Open Space Easement unto the said Grantee, its successors and assigns forever, to it and its own proper use and behoof.

IN WITNESS WHEREOF, the Grantor has hereunto set its hand and seal on this day of 20

Certifi	v atamps or POSTAL SERVICE* Certificate Of Mailing	To pay fee, affix stamps or meter postage here.
This Certificate of Mailing provides evidence that mail has been presented to USPS® for mailing.	This Certificate of Mailing provides evidence that mail has been presented to USPS This form may be used for domestic and international mail.	
This form may be used for domestic and international main. From: DC Surviev Associates, LLC	From: PC Survey Associates, LLC	
crate Meadow Road	63 Snake Meadow Road	<i>a</i> ¹
	Killingly, CT 06239	
Killingly, CT 06239		
Postmark Here	To:	Postmark Here
MICZ	CIL REALTY INCORPORATED CIL REALTY INCORPORATED 157 CHARTER OAK AVE 3RD FLOOR HARTFORD CT 06106	
	1	
	PS Form 3817, April 2007 PSN 7530-02-000-9065	
PS Form 3817 , April 2007 PSN 7530-02-000-9065		
Image: Mailing postal State Certificate Of To pay fee, affix stamps or Mailing meter postage here.	POSTAL SERVICE® Certificate Of Mailing	To pay fee, affix stamps or meter postage here.
This Certificate of Mailing provides evidence that mail has been presented to USPS® for mailing. This form may be used for domestic and international mail.	This Certificate of Mailing provides evidence that mail has been presented to USPS® for mailing. This form may be used for domestic and international mail.	
From: PC Survey Associates, LLC	From: PC Survey Associates, LLC	
63 Snake Meadow Road	63 Snake Meadow Road	
Killingly, CT 06239	Killingly, CT 06239	
To:		Postmark Here
JOHN F ENNIS & FREDERICK S EGGERS 289 PROVIDENCE RD BROOKLYN CT 06234	148 PRINCE HILL LLC 59 SOUTH ST BROOKLYN CT 06234	
]		
PS Form 3817, April 2007 PSN 7530-02-000-9065	PS Form 3817, April 2007 PSN 7530-02-000-9065	

To pay fee, affix stamps or meter postage here.	n -	Postmark Here	
Certificate Of Mailing		J65	U.S. POSTAGE PAID DANIELSON, CT JUL 0729 AMOUNT AMOUNT R2305H130608-08
Image: Construction of the state of the	From: PC Survey Associates, LLC 63 Snake Meadow Road Killingly, CT 06239	Te: MARK SIMONZI 25 TOWER HILL RD BRIMFIELD MA 01010 PS Form 3817 , April 2007 PSN 7530-02-000-9065	OOO Source and a second
To pay fee, affix stamps or meter postage here.		Postmark Here	9 0
Certificate Of Mailing		065	U.S. POSTAGE PAID DANIELSON. CT JUL 00233 AMOUNT AMOUNT R2305H130608-08
Image: Construction of the second construction of the seco	y be used for domestic and international mail. PC Survey Associates, LLC 63 Snake Meadow Road Killingly, CT 06239	To: BRITTANY L PERRY 158 MAIN ST PO BOX 682 PUTNAM CT 06260 PS Form 3817 , April 2007 PSN 7530-02-000-9065	BOOD .
This Certificate o	This form may be From: PC 63 Ki:	To: PS Form 38	

NORTHEASTERN CONNECTICUT COUNCIL OF GOVERNMENTS

ENGINEERING PLAN REVIEW PERTAINING TO A 3-LOT SUBDIVISION (Assessor's MAP 34, LOT 52) PRINCE HILL ROAD BROOKLYN, CT (July 12, 2020)

The comments contained herein pertain to my review of plans for a 3-lot residential subdivision on Prince Hill Road in Brooklyn, Connecticut, consisting of four (4) sheets, prepared for David P. Bell and Nancy M. Bell by PC Survey Associates, LLC and Killingly Engineering Associates, dated June 220. My comments are as follows:

Sheet 2 of 4 – Subdivision Map

1. It is recommended that the Conservation Easement boundary across Lot Nos. 2 and 3 be marked with durable aluminum signs, no less than 12" wide in any dimension, mounted on pressure treated posts at beginning and end points, angle points and intervals between said points at a distance of no more than 100' apart.

Sheet 3 of 4 – Lot Development Plan

1. Below the test hole data listing, there is a note that states "Additional percolation tests at depths above restrictive later to be performed on Lots 1 and 3 at the time of lot development." Why is this caveat here and is there a potential negative impact on building a house on this lot? Please explain.

Sheet 3 of 4 – E&S Controls / Details

- 1. In the "Curtain Drain Lot 3" detail, it is recommended that a detectable warning tape be placed over the PVC pipe to allow tracing once it is buried.
- Again, in the "Curtain Drain Lot 3" detail, how are the perforations to be oriented up or down? Also, the current Connecticut Department of Transportation publication is Form 818, thus the note should be corrected.

- 3. In the "Anti-Tracking Pad" detail, the note regarding special riprap, change Form 814 to Form 818 and recheck the Material Article to ensure the reference is correct in the latest Form.
- 4. In the "Residential Driveway Detail," it should be noted that side slopes in cut or fill shall be no steeper than 3H:1V and a reference be made to the Brooklyn Public Improvement Specifications.

General Comment

1. The signature and seal of the professional engineer is missing on the plans. Also, the soil scientist signature block is missing. These should be added to the plans.

By: 2020 Syl Pauley, Jr., P.E., NECCOG Regional Engineer

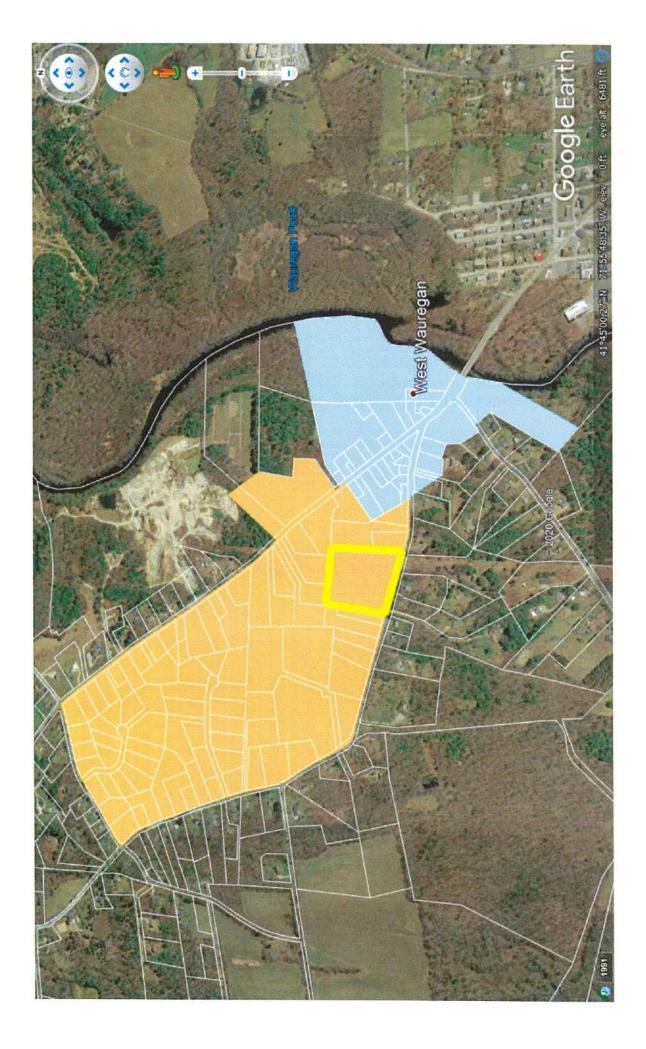
PLANNING AND ZONING COMMISSION

REQUEST FOR CHANGE IN ZONING BOUNDARY

Date July 20th 2000	FEE \$ 250.00 State Fee \$ 60.00
Application # $ZC 20 - \infty 2$	Check #
Public Hearing Date Commission Action Effective Date	
Name of Applicant KEITH A. CROSSMAN	Phone 860-420-8407
Mailing Address 340 CHRISTIAN HILL RO.	
Applicants Interest in the Property	
Property Owner BRUCE & CATHY CROSSMAN	
Mailing Address 242 CHRISTIAN HILL RD.	
MAP LOT MAP LOT MAP LOT More lots , repeat above on separate sheet	
ZONE: R10 R30 RA VCD NC RB	
REQUEST CHANGE:FROMTOREQUEST CHANGE:FROMTO	2
REQUEST CHANGE: FROMTO	
More changes, repeat above on separate sheet	
REASON FOR REQUEST: FAMILY WOULD LIKE TO HA	VE A SMALL HORSY FARM.

NOW MORE THAN EVER, A FAMILY NEEDS TO RAISE AND GROW FOOD ON THEIR PROPERTY.

Note: A petition may be filed at the Hearing by 20% or more of the area lots included in such a change within 500 ft of the property under Section 16.5 of the Zoning Regulations





PROPOSED PARKING EXPANSION "VACHON CHEVROLET"

CONSTRUCTION NOTES/GENERAL PROVISIONS

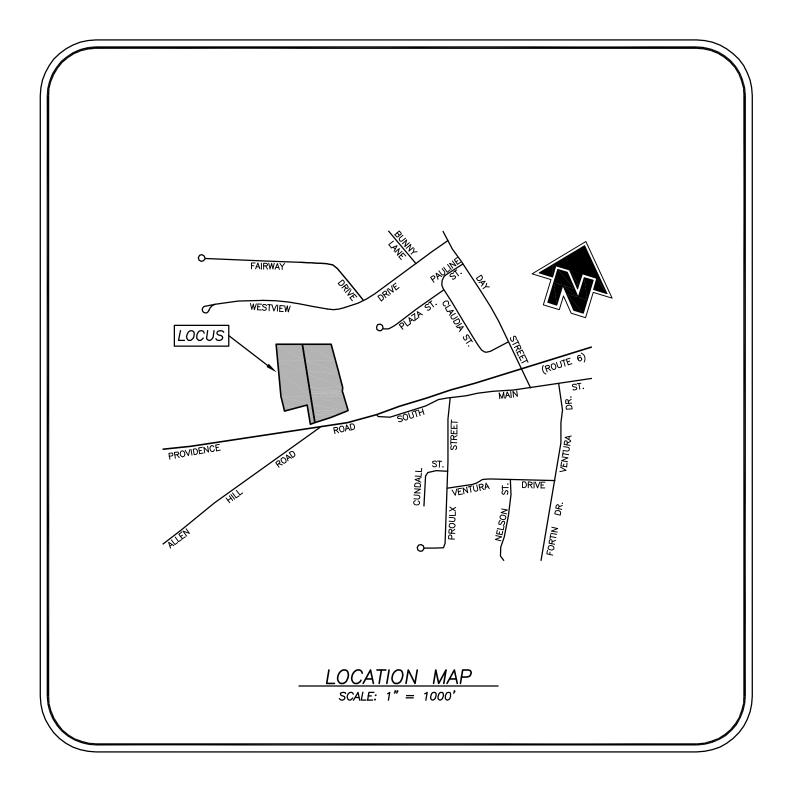
- 1. The locations of existing utilities are based upon visible field observations, record mapping and interviews with the property owner and abutting property owners. They are shown for informational purposes only. Contractor shall coordinate exploratory test hole excavation with the Engineer if necessary to verify and/or determine actual locations of some utilities & structures. It is the responsibility of the contractor to verify the location and elevation of all utilities. Contact "CALL BEFORE YOU DIG" at 1-800-922-4455, and obtain all applicable permits, prior to any excavation around utilities.
- 2. All existing site features not scheduled to remain shall be removed and disposed of in a proper manner, by the contractor.
- 3. All Materials and methods of construction shall conform to "State of Connecticut, Department of Transportation, Standard Specifications for Roads, Bridges and Incidental Construction, Form 816", and supplements thereto.
- 4. The Contractor shall obtain copies of all regulatory agency permits from the Owner prior to any site disturbance.
- 5. Unless otherwise noted on the plans, the contractor shall use the geometry provided on the construction plans. Benchmark information shall be provided to the contractor by the Owner or the Owner's surveyor. Any discrepancies between field measurements and construction plan information shall be brought to the attention of the Engineer or Surveyor immediately.
- 6. The Contractor shall not revise elevations or locations of items shown on the plans without written consent of the project Engineer or Surveyor.
- 7. The Contractor shall protect benchmarks, property corners, and other survey monuments from damage or displacement. If a marker needs to be removed, it shall be referenced by a licensed land surveyor and replaced as necessary by the same.
- 8. The Contractor shall be responsible for preparing and compacting base for proposed pavement. Owner shall provide general fill to establish subgrade - contractor shall spread and compact. Contractor shall provide, spread and compact required processed aggregate
- 9. The entire project site shall be thoroughly cleaned at the completion of the work. Clean all installed paved areas, accumulated silt and sediment, plus all adjacent areas affected by the construction activities as directed by the Owner or the jurisdictional Agency.

•	IRON PIN TO E
O	IRON PIN FOUR
	CONCRETE MO
CHD PNT	CHD MONUMEN
4	SIGN
Ø	UTILITY POLE
□св	CATCH BASIN
⊙ мн	MANHOLE
⊙ SMH	SANITARY SEWE
# [`]	INLAND WETLAN
100	EXISTING CONT
	PROPOSED CO
	SILT FENCE

APPROVED BY T PLANNING AND ZON		
		HE BROOKLYN INLAND S COMMISSION
CHAIRMAN	DATE	
xpiration date per Sec. 8.260 Connecticut General Statutes:		DATE

PROVIDENCE ROAD (ROUTE 6) BROOKLYN, CONNECTICUT

PREPARED FOR: VACHON BROOKLYN, LLC



PREPARED BY:



JANUARY 2020

BE SET JND DNUMENT FOUND NT POINT

VER MANHOLE NDS FLAG ITOURS ONTOURS

INDEX TO DRAWINGS

TITLE	SHEET No.
COVER SHEET	1 OF 5
EXISTING CONDITIONS MAP	2 OF 5
SITE DEVELOPMENT PLAN 1	3 OF 5
SITE DEVELOPMENT PLAN 2	4 OF 5
DETAIL SHEET	5 OF 5

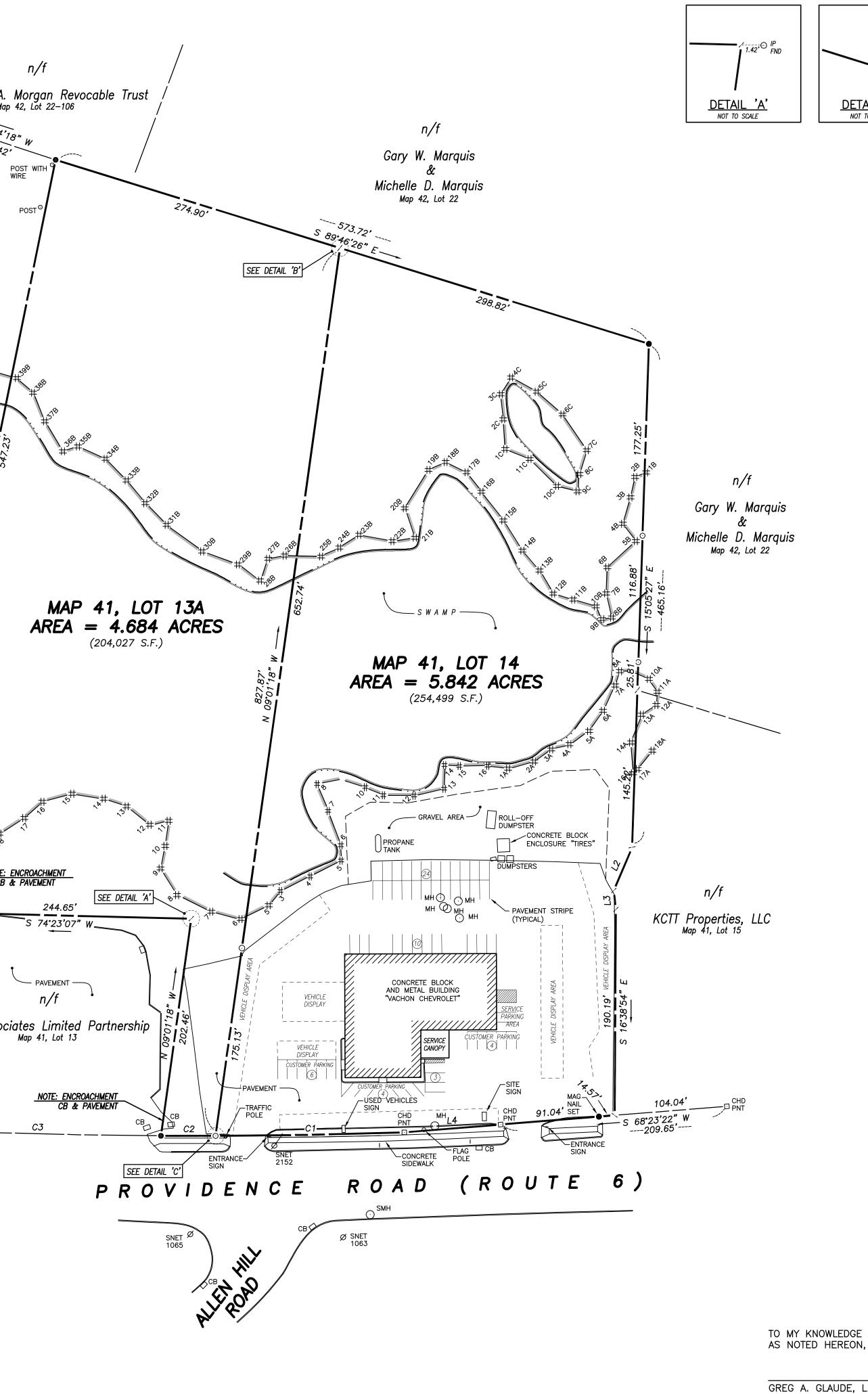
BEFORE YOU DIG CALL BEFORE YOU DIG AT LEAST TWO FULL BUSINESS DAYS **BEFORE DIGGING OR DISTURBING EARTH** DIAL 811 OR 1-800-922-4455

FOR REVIEW ONLY NOT FOR CONSTRUCTION

NORMAND THIBEAULT, JR., P.E. No. 22834 DATE

SHEET 1 OF 5 JOB NO: 19129

	60 0 0 60 60 60 60 60 6		Мар 88°54° 114.42
	CH = S / 1'56'28'' W CH = S / 3'04'30'' W CH = S / 4'2/ 174.32' 50.48' 224.16' LINE DATA L1 N 14'49'40'' W 34.19' L2 S 06'00'57'' W 43.34' L3 S 23'24'09'' E 17.56' L4 S 68'21'47'' W 89.41'	#* n/f	N 05'29'47" W 7400
		n/f	
MA	AP_REFERENCES:		#
1.	of Brooklyn — Brooklyn—Danielson Road — From the Old Pom Road — Easterly About 12,000 Feet — Route U.S.6. — Scale: 1" =	Town Infret <u>NOTE: ENCROACHMENT</u> 40'	₩ NOTE: CURB
2.	"Town of Brooklyn — Map Showing Land & Easement Acquired By — SAtate of Connecticut — From — Mildred Chase Hopkins — Reloca of Route U.S. 6 — Scale: 1" = 40' — Date: June 1953 — Town 19 — Project No. 43 — Serial No. 1 — Sheet 1 of 1 — Prepared	The ation 8' HIGH CHAIN LINK O FENCE	
3.	"Boundary Survey — property of — Stephen Castle — Route 6, Broo Conn. — Scale: 1" = 40' — Date: July 30, 1964 — Sheet 1 Prepared by: Morton S. Fine & Associates." On file in the Broo Land Records as Map Book 3 Page 52.	klyn, of 1 klyn	
4.	Connecticut — Scale: 1" = 20' — Date: March 19, 1982 — Prep By: Thomas A. Brennan, Jr." On file in the Brooklyn Land Rec	ared	Assoc
5.	"Subdivision Map — Prepared for — Gary D. Kuchy — Westview [Brooklyn, Connecticut — Scale: 1" = 80' — Date: June 16, 1 Revised to: 11/1/99 — Sheet 1 of 11 — Prepared by: J&D	999 Civil	
	Road — Route 6 — Brooklyn, Connectiuct — Scale: 1" = 40 Date: Nov. 2002 — Sheet No. 1 — prepared by: Archer Survey LLC." On file in the Brooklyn Land Records as Map Book 15 Page "Improvement Location Survey — Prepared for — Premier Chevr	o' — ving, CHD □ 90. PNT rolet	
	1" = 50' - Date: 10/12/2011 - Sheet 1 of 1 - Prepared	by:	
8.	KCTT Properties, LLC — Route #6 (Providence Road) — Brook Connecticut — Scale: 1" = 20' — Date: October 2016 — Rev	klyn, ised	
	APPROVED BY THE BROOKLYN PLANNING AND ZONING COMMISSION	FNDORSED BY THE BROOKLYN INLAND	
	CHAIRMAN DATE	WETLANDS COMMISSION	
	Expiration date per Sec. 8.26C, Connecticut General Statutes:	CHAIRMAN DATE	
	 1. 2. 3. 4. 5. 6. 7. 	Image: Solute IN FEET Image: Solute IN FEET <td< td=""><td>Control of the large product of the second of the sec</td></td<>	Control of the large product of the second of the sec



NO C THE

B A. GLAUDE, L.S. LIC. NO. 70191 DATE CERTIFICATION IS EXPRESSED OR IMPLIED UNLESS THIS MAP BEARS ORIGINAL SEAL AND SIGNATURE OF THE LAND SURVEYOR. DATE: 1/07/2020 DRAWN: AMR DATE: 1/07/2020 DRAWN: AMR SCALE: 1" = 60' DESIGN: NET SHEET: 2 OF 5 CHK BY: DWG. No: CLIENT FILE JOB No: 19129			
GLAUDE, L.S. LIC. NO. 70191 DATE SHEET: 2 OF 5 CHK BY: CERTIFICATION IS EXPRESSED OR IMPLIED UNLESS THIS MAP BEARS DWG. No: CLIENT FILE JOB No: 19129		DATE: 1/07/2020	DRAWN: AMR
CHK BY: CHK BY:		SCALE: 1" = 60'	DESIGN: NET
	GA. GLAUDE, L.S. LIC. NO. 70191 DATE	SHEET: 2 OF 5	СНК ВҮ:
URIGINAL SEAL AND SIGNATURE OF THE LAND SURVETOR.		DWG. No: CLIENT FILE	JOB No: 19129
	URIGINAL SEAL AND SIGNATURE OF THE LAND SURVETOR.		

Killingly, Connecticut 06241 (860) 779-7299 www.killinglyengineering.com

DATE

TO MY KNOWLEDGE AND BELIEF, THIS MAP IS SUBSTANTIALLY CORRECT

BROOKLYN, CONNECTICUT 114 Westcott Road P.O. Box 421

Killingly Engineering Associates Civil Engineering & Surveying

IMPROVEMENT LOCATION SURVEY SHOWING EXISTING CONDITIONS PREPARED FOR

VACHON BROOKLYN, LLC

PROVIDENCE ROAD (ROUTE 6)

5. North orientation, bearings and coordinate values shown are based on North American Datum of 1983 (NAD 83) and are taken from actual field measurements of CGS Random Points B9262 and B9264.

03/10/2020 PER SOIL SCIENTIST REPORT & STAFF COMMENTS

REVISIONS

DESCRIPTION

03/31/2020 PER NECCOG REVIEW

Map 41, Lot 13A = Vachon Brooklyn, LLC 957 Washington Street, Attleboro, MA 02703 Volume 632, Page 114 4. Wetlands shown were delineated in the field by Joseph Theroux, Certified Soil Scientist, in September 2019.

Map 41, Lot 14 = Vachon Brooklyn, LLC 957 Washington St., Attleboro, MA 02703 Volume 620, Page 163

3. Owner of record:

2. Zone = PC.

- Survey Type: Improvement Location Survey. - Boundary Determination Category: Dependent Resurvey.

- This survey conforms to a Class "A-2" horizontal accuracy.

This survey has been prepared pursuant to the Regulations of Connecticut State Agencies Sections 20-300b-1 through 20-300b-20 and the "Standards for Surveys and Maps in the State of Connecticut" as adopted by the Connecticut Association of Land Surveyors, Inc. on September 26, 1996;

NOTES:

<u>LEGEND</u> IRON PIN TO BE SET IRON PIN FOUND \odot CONCRETE MONUMENT FOUND $\overline{}$

SIGN

UTILITY POLE

CATCH BASIN

MANHOLE

CHD MONUMENT POINT

SANITARY SEWER MANHOLE

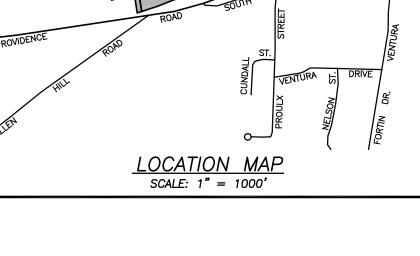
⊡ CHD PNT

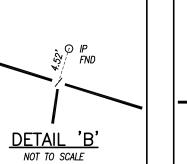
4

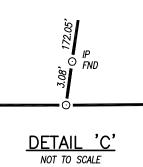
Ø

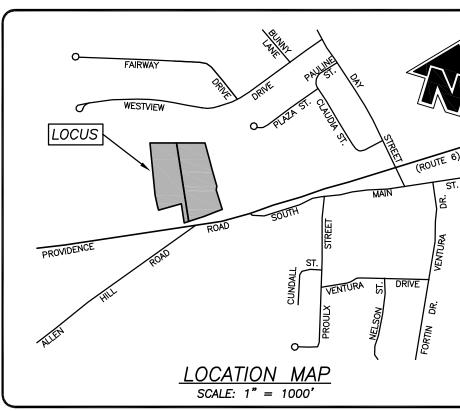
□св 🛈 мн

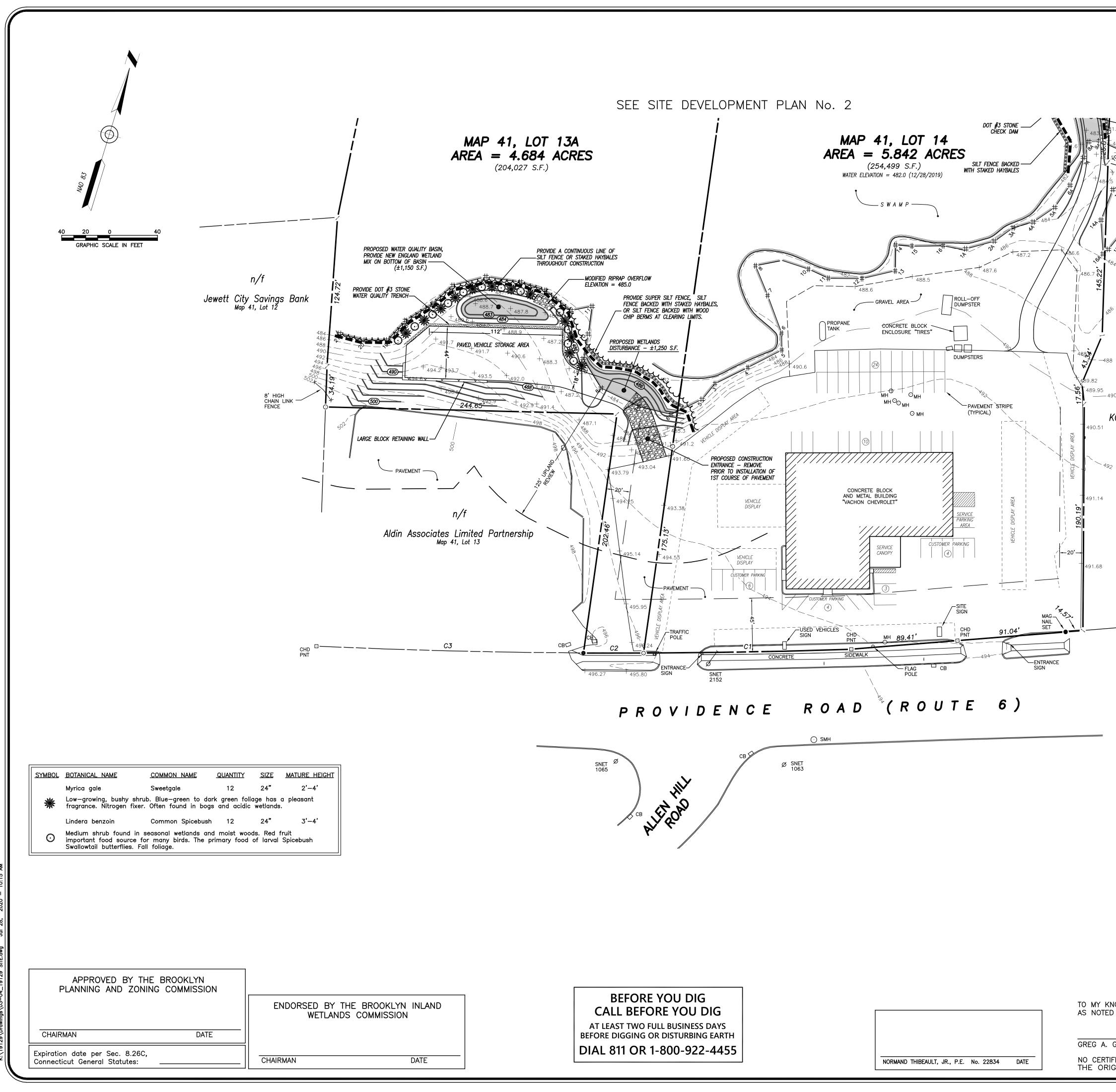
⊙ SMH





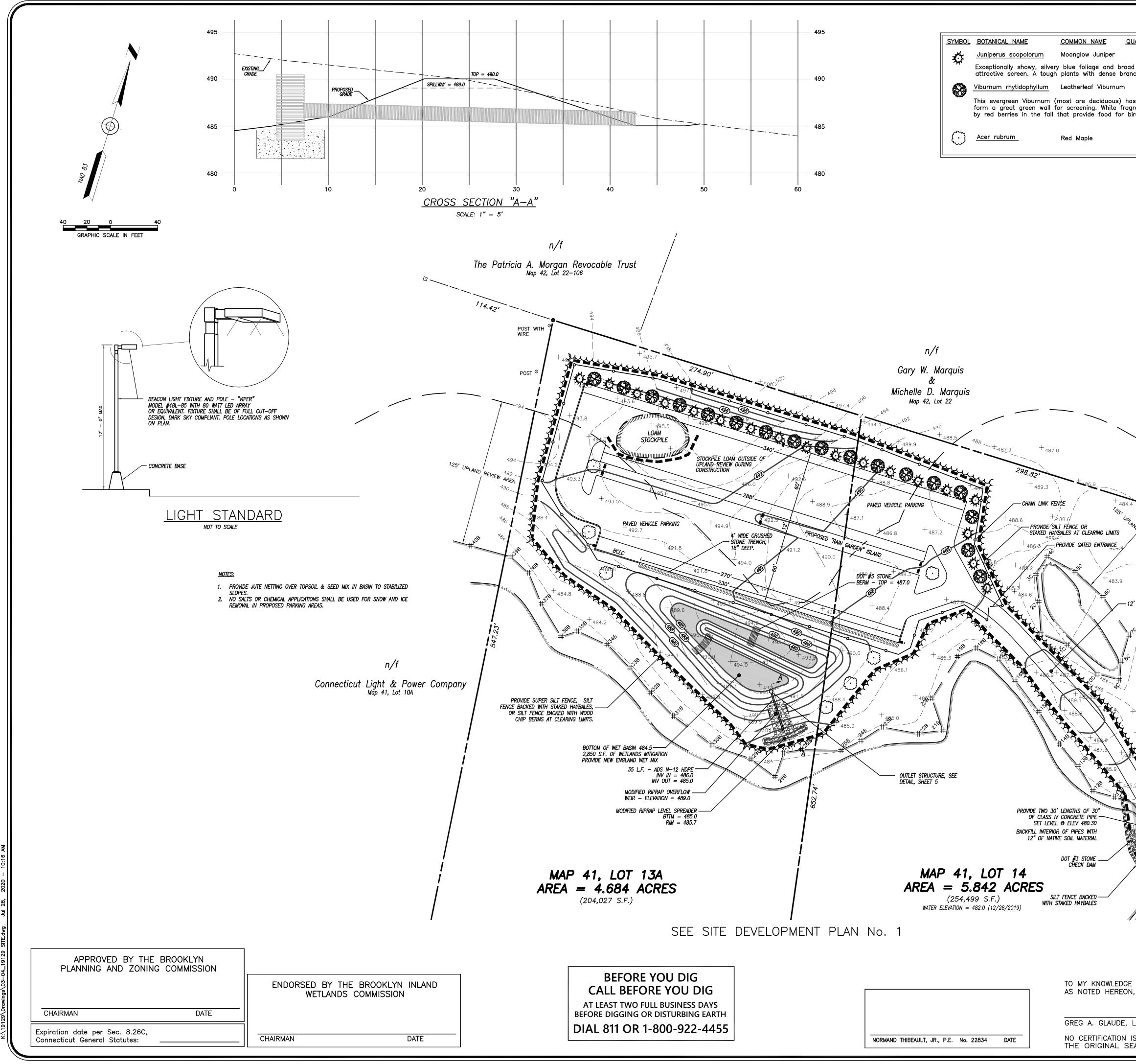






awinas\03-04_19129 SITE.dwa Jul 28. 2020 -

	CURVE DATA			
	R = 5680.00' R D = 1.45'30'' D L = 174.32' L CH = S 71.56'28'' W C	$\begin{array}{l} 22\\ 8 = 5680.00'\\ 0 = 0'30'33''\\ 5 = 50.48'\\ 24 = 5 73'04'3\\ 5 0.48' \end{array}$	D L 80" W CI	3 = 5680.00' = 2°15'41" = 224.18' H = S 74°27'37" W 24.16'
DISTURBANCE – ±1,860 S.F.				
32.4 井 ▼483.0, ►		LEGEND		
	•		IN TO BE	SET
184 # . 2 M	õ		IN FOUND	JEI
			ONUMENT	
.v ⁵	PNT I	SIGN		
,#**	Ø □св	UTILITY CATCH		
7	О мн	MANHOL		
	<u>О</u> SMH		Y SEWER	
	 100		G CONTOU	
		→ PROPOS	SED CONT	DURS
			NCE	
n/f				
TT Properties, LLC Map 41, Lot 15				
	<u>NOTES:</u> 1. This survey has	s been prepare	ed pursua	nt to the Regulations
	of Connecticut 20-300b-20 an	State Agencies d the "Standar	s Sections ds for Sur	20—300b—1 through veys and Maps in the
	State of Conn	necticut" as o	adopted	by the Connecticut September 26, 1996;
	— This survey o	conforms to a (Class "A—2	" horizontal accuracy.
	— Topographic vertical accu		form to c	ı Class "T−2", "V−2"
	– Survey Type:	•	Location S	urvey.
	2. Zone = PC.			
	3. Owner of record:			
	Map 41, Lot 14	= Vachon Brog 957 Washir Volume 620	ngton St.	, Attleboro, MA 02703 3
104.04 [°] CHD PNT	Map 41, Lot 134	957 Washir Volume 632	ngton St. 2, Page 11	, Attleboro, MA 02703 4
	4. Wetlands shown Certified Soil Sci	were delineated ientist, in Septe	l in the fie mber 201	eld by Joseph Theroux, Ə.
	based on North A	American Datum	of 1983 (ate values shown are NAD 83) and are taken ndom Points B9262 and
	6. Elevations showr			med datum. Contours
			-	Contour interval = $2'$.
	YOU DIG" at 1-8	800-922-4455	or 811.	contact "CALL BEFORE
	07/7/ (0000 0 0		-1.40	
	03/10/2020 P	PER NECCOG RE PER SOIL SCIENT	TIST REPOR	RT & STAFF COMMENTS
	DATE	REVISI	DESCRII ONS	PTION
			-	
	IMPROV	EMENT LO	CATIO	N SURVEY
	SITE D	EVELOPM	ENT PL	AN No. 1
		PREPARI	ED FOR	
	_	_	_	
	VACHON	N BRO	OKL	LYN, LLC
	510 DI	ROVIDENCE	ROAD	OUTE 6)
		ROVIDENCE I ROOKLYN, CO		
	Killingly Engineering Ass		-	
	1		Westcott Road	
	Ma M.	Η	P.O. Box 421 y, Connecticut (
		Killingly	360) 770 7000	
	ст	(8	860) 779-7299 inglyengineerir	g.com
	DATE: 1/07/2020	(8	inglyengineerir	AMR
DWLEDGE AND BELIEF, THIS MAP IS SUBSTANTIALLY CORRE HEREON, LAUDE, L.S. LIC. NO. 70191 DATE		(8	inglyengineerir	AMR NET



IAME	QUANTITY	<u>SIZE</u>	MATURE HEIGHT	
Juniper	18	5'	16'—20'	
	road pyramio pranching the		makes a most to 20'tall.	
Viburnur	n 17	3-4'	6'—10'	
eciduous) has this olive green leaves that y. White fragrant flowers in the spring followed food for birds.				
	5	3" Calipe	er 60'-80'	

484.4

₹484.0

- 12' WIDE ACCESS DRIVE

484.7 +

483.6

+ 485.3

485

∕ .#/

485.6

36.8

+486.5

PROPOSED WETLANDS DISTURBANCE – ±1,860 S.F.

E HEIGHT	CURVE DATA
²–20 ²	C1 C2 C3
a most all.	R = 5680.00' $R = 5680.00'$ $R = 5680.00'$ $D = 1.45'30''$ $D = 0.30'33''$ $D = 2.15'41''$ $L = 174.32'$ $L = 50.48'$ $L = 224.18'$
-10'	$CH = S 71^{\circ}56'28'' W CH = S 73^{\circ}04'30'' W CH = S 74^{\circ}27'37'' W$
hat followed	174.32' 50.48' 224.16'
-80'	LEGEND
	● IRON PIN TO BE SET
	• IRON PIN FOUND
	I SIGN
	ØUTILITY POLE ПСВ CATCH BASIN
	⊙ MH MANHOLE
	SILT FENCE
	NOTES:
	 This survey has been prepared pursuant to the Regulations of Connecticut State Agencies Sections 20-300b-1 through
	20-300b-20 and the "Standards for Surveys and Maps in the State of Connecticut" as adopted by the Connecticut
	Association of Land Surveyors, Inc. on September 26, 1996;
	 This survey conforms to a Class "A-2" horizontal accuracy. Topographic features conform to a Class "T-2", "V-2"
λ.	vertical accuracy.
\backslash	— Survey Type: Improvement Location Survey.
· · · · · · · · · · · · · · · · · · ·	2. $Zone = PC.$
\backslash	3. Owner of record:
١	Map 41, Lot 14 = Vachon Brooklyn, LLC 957 Washington St., Attleboro, MA 02703 Volume 620, Page 163
5	Map 41, Lot 13A = Vachon Brooklyn, LLC 957 Washington St., Attleboro, MA 02703
	Volume 632, Page 114 4. Wetlands shown were delineated in the field by Joseph Theroux, Certified Soil Scientist, in September 2019.
	5. North orientation, bearings and coordinate values shown are based on North American Datum of 1983 (NAD 83) and are taken from actual field measurements of CGS Random Points B9262 and
1	B9264. 6. Elevations shown are based on an assumed datum. Contours
/	shown are taken from actual field survey. Contour interval = 2'.
	 Before any construction is to commence contact "CALL BEFORE YOU DIG" at 1-800-922-4455 or 811.
SILT FENCE BACKED WITH STAKED HAYBALES	
n/f	03/31/2020 PER NECCOG REVIEW
, 	03/10/2020 PER SOIL SCIENTIST REPORT & STAFF COMMENTS DATE DESCRIPTION
Gary W. Marquis &	REVISIONS
a lichelle D. Marquis	
Map 42, Lot 22 '	IMPROVEMENT LOCATION SURVEY
DOT #3 STONE	SITE DEVELOPMENT PLAN No. 2
— СНЕСК ДАМ	PREPARED FOR
OSED WETLANDS RBANCE – ±1,860 S.F.	
	VACHON BROOKLYN, LLC
	512 PROVIDENCE ROAD (ROUTE 6)
	BROOKLYN, CONNECTICUT
	Killingly Engineering Associates
	114 Westcott Road

114 Westcott Road

P.O. Box 421

Killingly, Connecticut 06241 (860) 779-7299

www.killinglyengineering.com

DATE: 1/07/2020

SCALE: 1" = 40'

SHEET: 4 OF 5

DWG. No: CLIENT FILE

DRAWN: AMR

DESIGN: NET

СНК ВҮ: ———

JOB No: 19129

TO MY KNOWLEDGE AND BELIEF, THIS MAP IS SUBSTANTIALLY CORRECT

GLAUDE, L.S.	LIC.	NO. 70191	DATE
			S THIS MAP BEARS
IFICATION IS EX	TRESSED OR IN	IFLIED UNLES.	S INIS MAP DEARS
IGINAL SEAL /	AND SIGNATUF	RE OF THE I	LAND SURVEYOR.

EROSION AND SEDIMENT CONTROL PLAN:

- REFERENCE IS MADE TO:
- 1. Connecticut Guidelines for Soil Erosion and Sediment Control 2002 (2002 Guidelines).
- 2. U.S.D.A. N.R.C.S. Web Soil Survey

DEVELOPMENT CONTROL PLAN:

- 1. Development of the site will be performed by the Contractor, who will be responsible for the installation and maintenance of erosion and sediment control measures required throughout construction
- 2. The sedimentation control mechanisms shall remain in place from start of construction until permanent vegetation has been established. The representative for the Town of Brooklyn will be notified when sediment and erosion control structures are initially in place. Any additional soil & erosion control measures requested by the Town or its agent, shall be installed immediately. Once the proposed development, seeding and planting have been completed, the representative shall again be notified to inspect the site. The control measures will not be removed until this inspection is complete.
- 3. All stripping is to be confined to the immediate construction area. Topsoil shall be stockpiled so that slopes do not exceed 2 to 1. A hay bale sediment barrier is to surround each stockpile and a temporary vegetative cover shall be provided.
- 4. Dust control will be accomplished by spraying with water. The application of calcium chloride is not permitted adjacent to wetland resource areas or within 100' of these areas.
- 5. The proposed planting schedule is to be adhered to during the planting of disturbed areas throughout the proposed construction site.
- 6. Final stabilization of the site is to follow the procedures outlined in "Permanent Vegetative Cover". If necessary a temporary vegetative cover is to be provided until a permanent cover can be

SILT FENCE INSTALLATION AND MAINTENANCE:

- 1. Dig a 6" deep trench on the uphill side of the barrier location.
- 2. Position the posts on the downhill side of the barrier and drive the posts 1.5 feet into the around.
- 3. Lav the bottom 6" of the fabric in the trench to prevent undermining and backfill.
- 4. Inspect and repair barrier after heavy rainfall.
- 5. Inspections will be made at least once per week and within 24 hours of the end of a storm with a rainfall amount of 0.5 inch or greater to determine maintenance needs.
- 6. Sediment deposits are to be removed when they reach a height of 1 foot behind the barrier or half the height of the barrier and are to be deposited in an area which is not regulated by the inland wetlands commission
- 7. Replace or repair the fence within 24 hours of observed failure. Failure of the fence has occurred when sediment fails to be retained by the fence because:
- the fence has been overtopped, undercut or bypassed by runoff water - the fence has been moved out of position (knocked over), or
- the geotextile has decomposed or been damaged.

HAY BALE INSTALLATION AND MAINTENANCE:

- 1. Bales shall be placed as shown on the plans with the ends of the bales tightly abutting each
- 2. Each bale shall be securely anchored with at least 2 stakes and gaps between bales shall be wedged with straw to prevent water from passing between the bales.
- 3. Inspect bales at least once per week and within 24 hours of the end of a storm with a rainfall amount of 0.5 inches or greater to determine maintenance needs.
- 4. Remove sediment behind the bales when it reaches half the height of the bale and deposit in an area which is not regulated by the Inland Wetlands Commission.
- 5. Replace or repair the barrier within 24 hours of observed failure. Failure of the barrier has occurred when sediment fails to be retained by the barrier because: - the barrier has been overtopped, undercut or bypassed by runoff water.
- the barrier has been moved out of position, or - the hay bales have deteriorated or been damaged.

TEMPORARY VEGETATIVE COVER:

SEED SELECTION

rass species shall be appropriate for the season and site conditions. Appropriate species are outlined in Figure TS-2 in the 2002 Guidelines.

TIMING CONSIDERATIONS

Seed with a temporary seed mixture within 7 days after the suspension of grading work in disturbed areas where the suspension of work is expected to be more than 30 days but less than 1 year. SITE PREPARATION

Install needed erosion control measures such as diversions, grade stabilization structures, sediment basins and grassed waterways.

Grade according to plans and allow for the use of appropriate equipment for seedbed preparation, seeding, mulch application, and mulch anchoring.

SEEDBED PREPARATION

Loosen the soil to a depth of 3-4 inches with a slightly roughened surface. If the area has been recently loosened or disturbed, no further roughening is required. Soil preparation can be accomplished by tracking with a bulldozer, discing, harrowing, raking or dragging with a section of chain link fence. Avoid excessive compaction of the surface by equipment traveling back and forth over the surface. If the slope is tracked, the cleat marks shall be perpendicular to the anticipated direction of the flow of surface water.

If soil testing is not practical or feasible on small or variable sites, or where timing is critical, fertilizer may be applied at the rate of 300 pounds per acre or 7.5 pounds per 1,000 square feet of 10-10-10 or equivalent. Additionally, lime may be applied using rates given in Figure TS-1 in the 2002 Guidelines.

SEEDING

Apply seed uniformly by hand cyclone seeder, drill, cultipacker type seeder or hydroseeder at a minimum rate for the selected species. Increase seeding rates by 10% when hydroseeding. MUI CHING

Temporary seedings made during optimum seeding dates shall be mulched according to the recommendations in the 2002 Guidelines. When seeding outside of the recommended dates, increase the application of mulch to provide 95%-100% coverage. MAINTENANCE

Inspect seeded area at least once a week and within 24 hours of the end of a storm with a rainfall amount of 0.5 inch or areater for seed and mulch movement and rill erosion. Where seed has moved or where soil erosion has occurred, determine the cause of the failure.

Repair eroded areas and install additional controls if required to prevent reoccurrence of erosion.

DATE

Continue inspections until the grasses are firmly established. Grasses shall not be considered established until a ground cover is achieved which is mature enough to control soil erosion and to survive severe weather conditions (approximately 80% vegetative cover).

APPROVED BY THE BROOKLYN

PLANNING AND ZONING COMMISSION

CHAIRMAN	

Expiration date per Sec. 8.26C. Connecticut General Statutes:

DATE

PERMANENT VEGETATIVE COVER:

Refer to Permanent Seeding Measure in the 2002 Guidelines for specific applications and details related to the installation and maintenance of a permanent vegetative cover. In general, the following sequence of operations shall apply:

- 1. Topsoil will be replaced once the excavation and grading has been completed. Topsoil will be spread at a minimum compacted depth of 4".
- 2. Once the topsoil has been spread, all stones 2" or larger in any dimension will be removed as well as debris.
- 3. Apply agricultural ground limestone at a rate of 2 tons per acre or 100 lbs. per 1000 s.f. Apply 10-10-10 fertilizer or equivalent at a rate of 300 lbs. per acre or 7.5 lbs. per 1000 s.f. Work lime and fertilizer into the soil to a depth of 4".
- 4. Inspect seedbed before seeding. If traffic has compacted the soil, retill compacted areas.
- 5. Apply the chosen grass seed mix. The recommended seeding dates are: April 1 to June 15 & August 15 - October 1.
- 6. Following seeding, firm seedbed with a roller. Mulch immediately following seeding. If a permanent vegetative stand cannot be established by September 30, apply a temporary cover on the topsoil such as netting, mat or organic mulch

DEVELOPMENT SCHEDULE/SEQUENCE OF OPERATIONS: 1. Flag the limits of disturbance and schedule pre-construction meeting with Town of

2. Install the anti-tracking construction entrance

Brooklyn wetlands Agent.

- 3. Install temporary logging crossing (corduroy crossing or slash mat) in the area of
- the wetlands crossing to allow for logging access.
- 5. Install perimeter erosion and sedimentation controls in accordance with the site development plan.
- 6. Excavate for proposed stormwater basin; area shall be utilized for a temproary sedimentation basin during construction.
- 7. Chip brush and slash; stockpile chips for use on site or remove off site. 8. When all logging activities have been completed, remove temporary crossing and install proposed pipes; counter sink pipes a minimum of 12" and fill bottoms with native material.
- 9. Box out areas to be paved and stockpile topsoil in locations shown on the plans. Install erosion controls around stockpiles and apply temporary seeding and divert water around the perimeter of the stockpile.
- 10.Install and compact processed gravel for driveway and parking area base.
- 11. Remove tree stumps and dispose of at an approved disposal site. Alternatively, stumps may be chipped in place. No stumps shall be buried on site.
- 12. Make all required cuts and fills. Establish the subgrade for the driveway as required and install additional erosion controls as necessary and as shown on the plans.
- 13.Inspect perimeter erosion and sedimentation controls weekly and after rain events in excess of 0.5". Repair any damaged controls and provide additional erosion control devices as necessary to address areas of concentrated runoff that may develop as a result of the construction activities. The contractor shall review discharge conditions with the design engineer or the Town of Brooklyn prior to installing additional erosion controls. Apply water as necessary for dust control.
- 14.Install required utilities.
- 15. Prepare sub-base for driveway and remainder of the parking areas for final grading.
- 16.Place topsoil where required and install any proposed landscaping.
- 17. Remove anti- tracking construction entrance and install first course of pavement.
- 18. When the remainder of the site work is near completion, sweep all payed areas for the final course of paving. Inspect erosion controls and remove any accumulated sediment. Clean accumulate sediment from the stormwater basin, apply topsoil & seed, and cover with jute netting.
- 19. Install final course of pavement upon the completion of the final structure.
- 20. Fine arade, rake, seed and mulch to within 2' of the pavement.
- 21. Remove and dispose of all silt fence and hay bales after the site has been stabilized to the satisfaction of the Town of Brooklyn.

RESPONSIBLE PARTY FOR E&S MAINTENANCE:

Joe Simon Vachon Chevrolet 512 Providence Road Brooklyn, CT 06234 (401) 692-1459

WETLAND SEED MIX FOR WETLANDS MITIGATION

The New England Wetmix (Wetland Seed Mix) contains a wide variety of native seeds that are suited to moist ground as found in most wet meadows, scrub shrub, or forested wetland basins not generally under standing water. The seeds will not germinate under inundated conditions. If planted during the fall months, the seed mix will germinate the following spring. During the first season of growth, several species will produce seeds while other species will This mix is comprised of the wetland species most likely to grow in created/restored wetlands and should produce more than 75% ground cover in two full growing seasons.

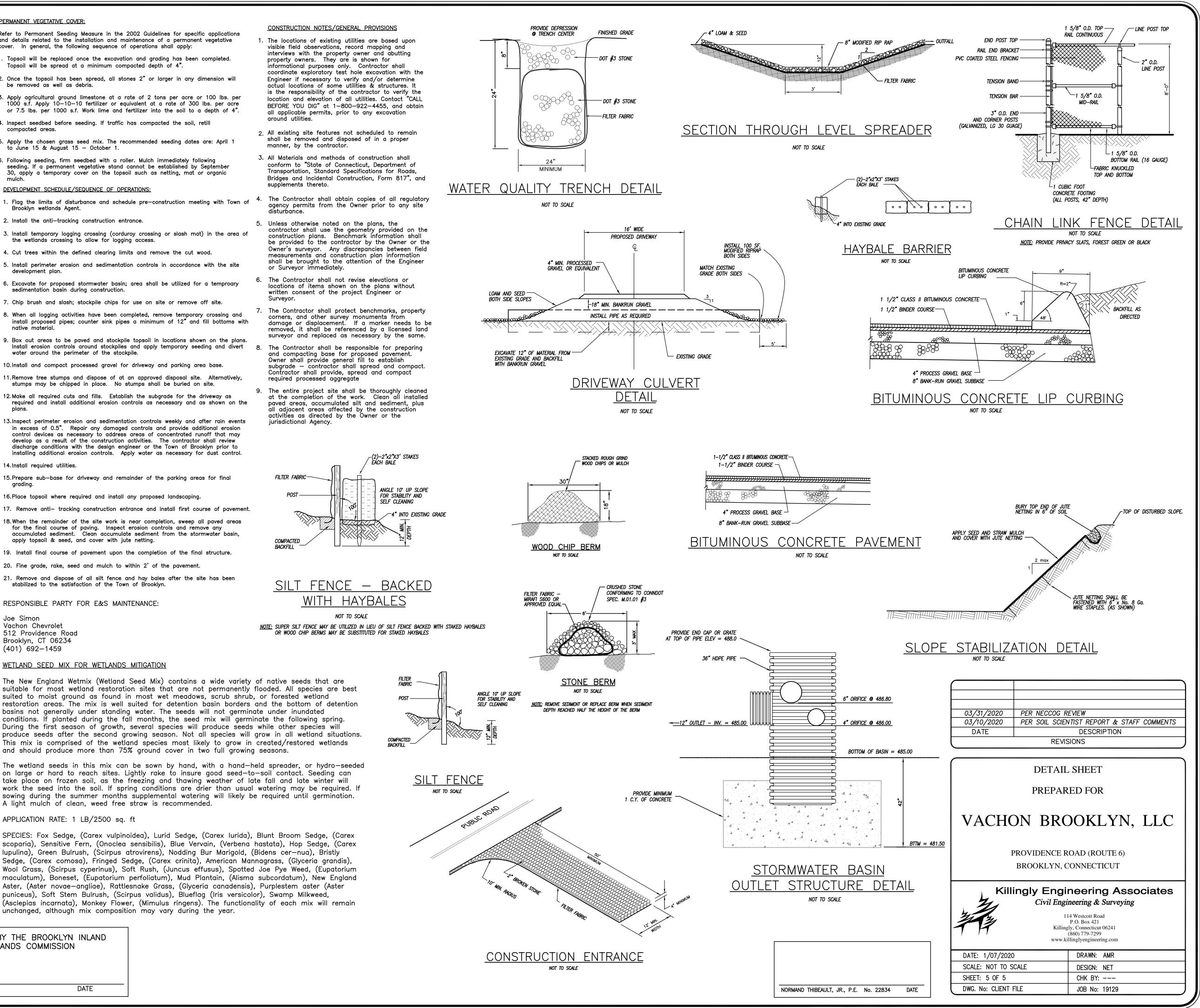
on large or hard to reach sites. Lightly rake to insure good seed—to—soil contact. Seeding can take place on frozen soil, as the freezing and thawing weather of late fall and late winter will sowing during the summer months supplemental watering will likely be required until germination. A light mulch of clean, weed free straw is recommended.

APPLICATION RATE: 1 LB/2500 sq. ft

lupulina), Green Bulrush, (Scirpus atrovirens), Nodding Bur Marigold, (Bidens cer-nua), Bristly Sedge, (Carex comosa), Fringed Sedge, (Carex crinita), American Mannagrass, (Glyceria grandis), Aster, (Aster novae-angliae), Rattlesnake Grass, (Glyceria canadensis), Purplestem aster (Aster puniceus), Soft Stem Bulrush, (Scirpus validus), Blueflag (Iris versicolor), Swamp Milkweed, (Asclepias incarnata), Monkey Flower, (Mimulus ringens). The functionality of each mix will remain unchanged, although mix composition may vary during the year.

ENDORSED BY THE BROOKLYN IN	ILAND
WETLANDS COMMISSION	

CHAIRMAN





PLANNING AND ZONING COMMISSION TOWN OF BROOKLYN CONECTICUT

Received Date _____

Application #SP<u>10-002</u> Check #_____

APPLICATION FOR SPECIAL PERMIT

Name of Applicant VACION BROOKLYN, LLC Phone 401-692-1459
Mailing Address 957 WASHINGTON ST ATTLEBORD, MA Phone
02703
Name of Engineer/Surveyor KILLINGLY ENGINGERING ASSUCIATES
Address PO BOD 421 KILLINGLY CT 06241
Contact Person NORMOND THIBERULT, JR Phone 779-7299 Fax
Name of Attorney
Address
Phone Fax
Property location/address PROVIDENCE ROAD (RIE-6)
Map#_41_Lot#13A\$14_Zone_PCTotal Acres_10.526
Sewage Disposal: Private Public <u>×</u> Existing <u>×</u> Proposed
Water: Private Public_X Existing_X Proposed
Proposed Activity CONSTRUCTION OF (2) 16 WINE ACCESS DRIVES
TO ACCESS PROPOSED NEW VEHICLE STORAGE WITS.

Compliance with Article 4, Site Plan Requirements

Is parcel located within 500 feet of an adjoining Town?_____

The following shall accompany the application when required:

12.1 Erosion and Sediment Control Plans

The owner and applicant hereby grant the Brooklyn Planning and Zoning Commission, the Board of Selectman, Authorized Agents of the Planning and Zoning Commission or Board of Selectman, permission to enter the property to which the application is requested for the purpose of inspection and enforcement of the Zoning regulations and the Subdivision regulations of the Town of Brooklyn

Date 7/28/20 Date 7/28/20 Applicant: ____ Owner:

*Note: All consulting fees shall be paid by the applicant

PLANNING AND ZONING COMMISSION TOWN OF BROOKLYN

CONECTICUT

Received Date _____ Action Date_____ Application #SPR_____ Check#_____

APPLICATION FOR SITE PLAN REVIEW

Name of Applicant VACHON BROOKLYN, LLC	Phone 401-692-1459
Mailing Address 957 WASHINGTON ST, ATTUEBORD,	MA Phone
4	02703
Name of Owner <u>SAME</u>	
Mailing Address	Phone
Name of Engineer/Surveyor KILLINGLY ENINEBRISH A	SSOCIATES
Address PO BOX 421 KILLINGLY CT 61	10241
Contact Person NORMAND THIBGAULT, JL Phone 779	-7299 Fax
Property location/address PRUVINENCE Rund (RT Map # 41 Lot # 13A Zone PC Total Acres 10.52	E.6)
Proposed Activity CONS PRUCTUD OF (2) 16' WI	DE ACCESS ORIUNE
TU ACCESS PROPOSED NEW VEHICLE STO	DRAGE /UTS
Change of Use: Yes No 📯 If Yes, Previous Use	
Area of Proposed Structure(s) or Expansion Z, L9 A C	
Utilities - Septic: On Site Municipal <u>></u> Existing Water: Private Public <u>></u> Existing	Proposed Proposed
Compliance with Article 4, Site Plan Requirements	
The following shall accompany the application when required:	
Fee\$	
Variances obtained	Date
The owner and applicant hereby grant the Brooklyn Planning an Selectman, Authorized Agents of the Planning and Zoning Comr to enter the property to which the application is requested for the enforcement of the Zoning regulations and the Subdivision regul	nission or Board of Selectman, permission ne purpose of inspection and
Applicant:	Date 7/28/20
Owner:	Date $7/28/20$ Date $7/28/20$

*Note: Any consulting fees will be paid by the applicant

LIST OF AJACENT LAND OWNERS INCLUDING ACROSS THE STREET BS OF 7/28/2020 GIS

Vachon Brooklyn, LLC Vachon Chevrolet Providence Road (Route 6) Brooklyn, CT

Job No. 19129

MAP//LOT	NAME
41//13	ALDIN ASSOCIATES LIMITED PARTNERSHIP 77 STERLING ROAD EAST HARTFORD, CT 06108
41//12	JEWETT CITY SAVINGS BANK PO BOX 335 JEWETT CITY, CT 06351-0335
41//10A	CONNECTICUT LIGHT & POWER CO PO BOX 270 HARTFORD, CT 06141-2335
42/ / 22-106	MORGAN THE PATRICIA A REVOCABLE TRUST 49 WESTVIEW DR BROOKLYN, CT 06234
42/ / 22	MARQUIS GARY W & MICHELLE D 43 WESTVIEW DR BROOKLYN, CT 06234
41//15	KCTT PROPERTIES LLC C/O KENNETH CARDINAL 520 PROVIDENCE RD BROOKLYN, CT 06234
41//108	CASEY BRIAN & ETHIER EILEEN 9 ALLEN HILL RD BROOKLYN, CT 06234-0156
41//109	CASEY BRIAN M 9 ALLEN HILL RD BROOKLYN, CT 06234-0156



Joseph R. Theroux

~ Certified Forester/ Soil Scientist ~ Phone 860-428-7992~ Fax 860-376-6842 P.O. Box 32, Voluntown, CT. 06384 Forestry Services ~ Wetland Impact Assessments Wetland Delineations and Permitting ~ E&S/Site Monitoring Wetland Function & Value Assessments

3/5/20

Killingly Engineering Associates P.O. Box 421 Dayville, CT. 06241

Re: Wetland function/value and impact assessment report for proposed parking expansion for Vachon Chevrolet, Providence Road, Brooklyn, Connecticut.

Dear Mr. Glaude,

At your request, I have reviewed the site plans entitled: "PROPOSED PARKING EXPANSION, "VACHON CHEVROLET" PROVIDENCE ROAD (ROUTE 6) BROOKLYN CONNECTICUT, dated 1/7/2020 and the above referenced property for the purposes of assessing the wetland functions and values and potential impacts to the inland wetlands and watercourses in proximity to the proposed parking area expansion.

The wetland function and value assessment was conducted on 2/26/20.

Existing Conditions

The property composed by two separate lots is 10.52 acres in size and is located on the north side of Providence Road, (Route 6), in Brooklyn, CT.

The southeast portion of the site is occupied by the car dealership with both paved and gravel parking areas. The remaining portion of the property is occupied by a large palustrine forested/scrub-shrub wetland & watercourse complex and adjacent forested uplands.

Upland Review Areas

The 125 foot upland review area around the delineated forested/scrub-shrub wetland/watercourse is vegetated in the overstory with a mix of white pine and mixed hardwoods in the sawtimber and polewood size classes. The mixed hardwoods include white and scarlet oaks, and red maple.

The understory is comprised of polewood and saplings in these species as well as shrub species such as highbush blueberry. Herbaceous vegetation includes hay scented ferns and miscellaneous grasses.

Wetlands

A palustrine forested/scrub-shrub wetland/watercourse was delineated in the central portion of the property. (See wetland delineation report). The wetland was inundated on the date of the delineation, (11/14/19) and the assessment, (2/26/20).

This area has formed due to the presence of a perched or seasonal ground water table that provides the hydrology to allow it to remain inundated throughout the year.

The wetland/watercourse is vegetated around its perimeter with scarlet oaks, white pine and red maple in the sawtimber size classes.

The majority of this wetland/watercourse is densely vegetated with red maple saplings and typical wetland shrub species such as highbush blueberry, speckled alder, sweet pepperbush, winterberry and spicebush.

Herbaceous vegetation included sphagnum moss, sensitive & cinnamon ferns, sedges, rushes, skunk cabbage, tussock sedges and misc. grasses. Floating duckweed was also noted in one area.

Wildlife tracks/sign found and directly observed in and adjacent to the wetland/watercourse included mammals and bird species such as: white tailed deer, eastern coyote, red tailed fox, raccoon gray & red squirrels, red tailed hawk, American crow, red wing blackbird, and numerous songbird species.

Due to the time of year, no amphibians or reptiles were observed although undoubtedly the main wetland/watercourse serves as habitat for numerous species.

A small depressed area containing wetland soils was also delineated in the northeast portion of the property, (delineated by the "C" series flags). This area was most likely a historic excavation, in which these wetland soils have formed due to prolonged wetness.

The perimeter of this area is vegetated in the overstory with red maple sawtimber and polewood, and the understory is comprised of shrubs such as highbush blueberry, and speckled alder. Herbaceous vegetation included sensitive and cinnamon ferns. Sedges were found within the inundated portion of the wetland.

It is my opinion that this small wetland may possibly serve as vernal habitat, although no wood frogs, salamanders or egg masses were found on the date of the assessment, (2/25/20).

Wetland Functions and Values

The forested/scrub-shrub wetland/watercourse, and the small wetland were inspected to determine wetland functions and values utilizing the Army Corps. Of Engineers methodology as outlined in "The Highway Methodology Workbook Supplement".

This methodology recognizes 8 separate wetland functions: groundwater recharge/discharge, floodflow alteration/storage, fish/shellfish habitat, sediment/toxicant/pathogen retention, nutrient removal/retention/transformation, production export, sediment/shoreline stabilization and wildlife habitat. The 4 wetland values include: recreational value, educational/scientific value, uniqueness/heritage value and threatened/endangered species habitat.

For each wetland function or value to be determined, 2 to 31 different considerations/or qualifiers are considered as rationale to apply or eliminate that specific function or value.

Palustrine forested/scrub-shrub wetland/watercourse functions:

The following is a list of the wetland functions exhibited by this wetland/watercourse and their descriptions:

Floodflow alteration: the large wetland/watercourse exhibits flood storage potential due to the flat topography, and valuable properties, structures and resources are located adjacent to the wetland.

Ground water recharge and discharge: Ground water recharge function is possible due to the perched water table being trapped and slowly infiltrating during dry season. This is a primary function of this wetland.

Sediment/toxicant retention: herbaceous vegetation, shrubs and flat topography in the wetlands can effectively trap sediments/toxicants from surface flows from the adjacent topography and gravel parking areas.

Nutrient removal/retention: herbaceous and shrub vegetation in the wetlands can effectively trap and utilize potential nutrients before reaching watercourses. Nitrogen fixing bacteria in wetland soils also trap nitrogen. Although with no current sources of nutrients present, this wetland has little opportunity to provide this function.

Production export: numerous tree, shrub and herbaceous plant species in the wetlands provide food, berries and seeds for wildlife. Amphibians provide food for birds and mammals.

Sediment and shoreline stabilization: Roots from herbaceous grasses and plants, shrub species and trees found in wetlands bind and stabilize soils which helps prevent erosion along steeper edges of wetlands. Although with no significant currents or shoreline waves, this wetland/watercourse has little opportunity to provide this function.

Wildlife habitat: Numerous amphibians, reptile, mammal, and bird species inhabit this wetland. The wetland and upland riparian zones adjacent to the wetland serve as wildlife habitat is another primary function of this wetland.

This wetland did not exhibit the wetland functions of fish habitat due to the lack of significant deep water habitat areas capable of sustaining fish.

Palustrine forested Scrub-shrub Wetland/Watercourse Values

The following wetland values were exhibited by this wetland/watercourse:

Educational/scientific value: this wetland/watercourse is relatively undisturbed, contains multiple wetland classes, and is considered as valuable wildlife habitat, although with no public access on this property, this wetland has little opportunity to provide this value.

Uniqueness/heritage value: this wetland/watercourse serves an important role in the ecological system of the area, it is a typical wetland class for the area, and serves as valuable wildlife habitat.

Visual/aesthetic value: the wetland/watercourse is visible from multiple viewing locations, it contains a diversity of vegetation that turns vibrant colors during different seasons, it is considered valuable wildlife habitat, and is not significantly disturbed.

This wetland/watercourse did not exhibit the value of threatened/endangered species habitat as the site was not shown within the shaded areas on the current natural diversity database maps.

"C Series" Wetland Functions:

The following is a list of the wetland functions exhibited by this wetland and their descriptions:

Ground water recharge and discharge: Ground water recharge function is possible due to the perched water table being trapped and slowly infiltrating during dry season. This is a primary function of this wetland.

Wildlife habitat: It is possible that amphibians, reptile, mammal, and bird species inhabit this wetland. The wetland and upland riparian zones adjacent to the wetland serve as wildlife habitat.

This wetland did not exhibit the wetland functions of floodflow alteration, sediment/toxicant retention, nutrient removal/retention, production export, sediment & shoreline stabilization and fish habitat due to the lack of floodwater storage capacity, its small area, lack of dense vegetation, lack of significant deep water habitat areas capable of sustaining fish, and it is not associated with stream flows or a large body of water.

"C Series" Wetland Values

The following wetland values were exhibited by this wetland:

Educational/scientific value: this wetland is relatively undisturbed, and is considered as wildlife habitat, although with no public access on this property, this wetland has little opportunity to provide this value.

Uniqueness/heritage value: this wetland serves an important role in the ecological system of the area, it is a typical wetland class for the area, and serves as wildlife habitat.

This wetland did not exhibit the visual/aesthetic value as it is not visible to the public, and does not contain vegetation that turn vibrant colors. It does not exhibit the value of threatened/endangered species habitat as the site was not shown within the shaded areas on the current natural diversity database maps.

Potential wetland impacts

The project plans and site were reviewed to assess the potential impacts to the wetlands from the proposed parking area expansion.

On the two parcels, an expansion of the existing parking areas is proposed, one area in the northern portion of both of the lots, and one in the southern portion of lot 13A.

Northern parking area:

In order to access the uplands in the northern portion of the parcels, a 1,860 square foot direct wetland disturbance is proposed for the 12 foot wide paved access drive. This will consist of excavation and installation of two 30 inch diameter class IV concrete pipes which will be filled along the bottom with native soil material.

Within the majority of the 125 foot upland review area and remaining uplands, the 12 foot wide access drive and a 340 foot long by 60 foot wide paved parking area is proposed with a storm water treatment basin located to the south of the parking area. In the bottom of the storm water basin, a 2,850 square foot wetland mitigation is also proposed. This area is designed to have a wet bottom which will fluctuate with the existing water table and will be seeded in with New England Wetmix.

The clearing limits and E&S measures shown on the plans vary from approx. 40 feet in width to immediately adjacent to the wetlands.

The topsoil stockpile is shown a reasonable distance from the wetlands and silt fencing is shown along the southern side.

Southern parking area:

In order to access the proposed 112 foot long by 44 foot wide paved parking area, a 1,250 square foot direct wetland disturbance is proposed for the construction of the access road.

To the north of the paved parking area, a storm water treatment basin is shown, and in the bottom of the basin a 1,150 square foot wetland mitigation is proposed. This area is also designed to have a wet bottom which will fluctuate with the existing water table and will be seeded in with New England Wetmix.

Also shown on the project plans are proposed plantings of common spicebush and sweetgale shrubs along the northern edge of the storm water treatment basin, to help revegetate and stabilize the side slopes.

The clearing limits and E&S measures on the plans for the most part are depicted immediately adjacent to the wetlands.

No topsoil stockpile is shown for this small construction area so I would assume that the topsoil will be hauled off site, or stored elsewhere on site, preferably with silt fencing around the perimeter.

E&S Measures:

The submitted project plans show the proposed E&S measures around the perimeter of the clearing limits adjacent to the wetlands as silt fencing and/or staked hay bales.

It would be my recommendation that the E&S measures be installed as soon as possible after the initial timber cutting and before the stumping and topsoil removal operation. It is during this phase where the most likely opportunity will occur for erosion and sedimentation. In some areas the slopes adjacent to the wetlands are steep, and the excavation, filling and grading are proposed directly adjacent to the wetlands.

Along the clearing limits adjacent to the wetlands, I would recommend either super silt fencing or silt fencing backed by staked hay bales should be proposed and implemented. This silt fencing will also prevent reptiles and amphibians from entering the excavation areas.

I would recommend that the storm water basins be constructed first before the remaining areas so they can serve as temporary sediment basins until the parking areas are constructed.

I would also recommend that E&S inspections be conducted on a frequent basis during the land clearing/stumping/topsoil stripping phases, and prior to significant storm events.

Direct wetland impacts:

The combined direct wetland disturbance for both of the wetland crossings totals 3,110 square feet. In this area all the specifically listed wetland functions and values for each wetland will be negated.

It is my opinion however, that the proposed 4,000 square foot wetland mitigation will compensate for this loss.

Potential short term impacts:

The potential short term impacts associated with the land clearing, stumping, top soil stripping and construction would be limited to potential sediment discharges during significant storm events.

Provided that the proposed/recommended E&S measures/inspections are correctly implemented and maintained throughout the project timeframe, the disturbance directly

adjacent to the wetlands will not significantly impact the wetlands or their existing functions due to erosion and sedimentation. Once the top soils are removed, the well-drained, sandy/gravely soils will allow for good infiltration of storm water runoff until the construction is complete.

The quick and permanent establishment of vegetation in the disturbed areas is crucial to the prevention of erosion. To minimize the potential for these impacts, E&S control measures have been incorporated into the project plans on sheet 5 of 5.

Potential long term impacts:

Wetland hydrology

I see no direct or long term impacts to the wetland hydrology as a result of the proposed access roads, parking areas or storm water treatment basins. As the access drives and parking areas are paved, storm water runoff will be an input to the existing hydrology, through some minor overland flow, but mostly through the storm water basins, as ground water recharge or as direct discharge during significant storm events after treatment.

Water quality:

Due to the incorporation of the paved parking surfaces, stone water quality trenches, storm water treatment basins, and some direct infiltration of storm water in the well-drained, sandy, gravelly soils, I see no significant or adverse impacts to the existing water quality of the wetlands from storm water discharges.

Adjacent upland wildlife habitat

Potential long term impacts to the upland habitat from the project would include the loss of a significant portion of the URA serving as riparian zones and upland wildlife habitat adjacent to the wetlands. This intrusion will force wildlife into the narrow vegetated corridor in and around the wetlands during and after the construction timeframe, and into other areas where the uplands are not disturbed. However, because this vegetated wildlife corridor is not proposed to be totally cleared and still exists in minimal widths in some areas, the wetlands and adjacent riparian zone will still provide for some wetland function and wildlife habitat.

It is my opinion that the proposed 4,000 square foot wetland mitigation will help compensate for these impacts to the upland/riparian habitat.

In summary, the design of the project implements features intended to minimize or eliminate potential impacts to the wetlands such as storm water runoff, significant loss of wetland habitat, and erosion and sedimentation associated with construction activities.

I feel these proposed measures are adequate to protect the wetlands provided that the recommended erosion and sedimentation control features are implemented and maintained throughout the excavation and reclamation timeframe.

The construction of the proposed 4,000 square foot wetland mitigation will assist in the remaining wetlands ability to provide the same wetland functions and values they currently provide.

If you have any questions concerning the site assessment or this report, please feel free to contact me.

Sincerely,

ren. Ele

Joseph R. Theroux Certified Forester and Soil Scientist Member SSSSNE, NSCSS, SSSA



antional - Anabalant - Contentury - Conspiler - Constitute - Marrietona - Realing y - providente Secretoris - Contente - Contente - Morres - Morreson - Venero - Venero - Venero - Venero - Venero - Venero - Ve

March 23, 2020

Ms. Jana Roberson, AICP Director of Community Development / Town Planner Town of Brooklyn 5 Wolf Den Road P.O. Box 356 Brooklyn, CT 06234

SUBJECT: Proposed Parking Expansion Vachon Chevrolet Assessor's Map 41, Lot Nos. 13A & 14 Providence Road (Route 6) Brooklyn, Connecticut

Dear Ms. Roberson:

As you requested, I have reviewed the devloper's consulting engineer's plans for the above captioned project. A copy of my comments are enclosed pertaining to my review of the plans, consisting of five sheets, entitled "Proposed Parking Expansion, 'Vachon Chevrolet', Providence Road (Route 6), Brooklyn, Connecticut, Prepared for Vachon Brooklyn, LLC.," which were created by Killingly Engineering Associates, dated January 2020 with revision date of March 10, 2020.

If you should have any questions, please do not hesitate to email me at syl.pauley@neccog.com.

Sincerely, Syl Pauley, Jr., P.E. NECCOG Regional Engineer

SP/s CC: File JRLtr_ProposedParkingExpansionVacbonChevroler_Xmit 03202020 Review Cmtr.doc

NORTHEASTERN CONNECTICUT COUNCIL OF GOVERNMENTS

ENGINEERING PLAN REVIEW PERTAINING TO PROPOSED PARKING EXPANSION VACHON CHEVROLET (Assessor's MAP 41, LOTS 13A & 14) PROVIDENCE ROAD (ROUTE 6) BROOKLYN, CT (March 20, 2020)

The comments contained herein pertain to my review of plans, consisting of five sheets, entitled "Proposed Parking Expansion, 'Vachon Chevrolet', Providence Road (Route 6), Brooklyn, Connecticut, Prepared for Vachon Brooklyn, LLC.," prepared by Killingly Engineering Associates, dated January 2020 with revision date of March 10, 2020.

SHEET 2 OF 5 – EXISTING CONDITIONS

1. The northing and eastling coordinates should be noted for CGS Random Points B9262 and B9264. It would also be helpful to include a large scale diagram as to where these points are relative to the project.

SHEET 3 OF 5 - SITE DEVELOPMENT PLAN NO. 1

- 1. A construction entrance symbol is drawn at the entrance to the new "paved vehicle storage area." However, a note should be included to indicate that this will be removed at the time the first course of paving is installed for the vehicle storage area. Additionally, it would be helpful for this explanation to be included in Note 17 under "Development Schedule/Sequence of Operations" that appears on Sheet 5 of 5 of the plan set.
- The note "Silt Fence Backed with Staked Haybales or Wood Chip Berms" should read the same as the note on Sheet 4 of 5, "Provide Super Silt Fence, Silt Fence Backed with Staked Haybales, or Silt Fence Backed with Wood Chip Berms at Clearing Limits," for consistency.

SHEET 4 OF 5 - SITE DEVELOPMENT PLAN NO. 2

- 1. Proposed slopes in the detention basin range from 1:1 to 3:1. It is recommended that slope be uniform and that no slopes be steeper than 3:1 to reduce the tendency of soil erosion
- 2. Pedestal lighting, with dual light fixtures, is shown at three (3) locations in the middle of the proposed vehicle parking area. No description of the lighting assembly (pedestal height, pedestal base, full cutoff design, wattage, etc.) can be found in the plans under review. This is important since there is a house on adjacent Lot No. 22 that is not too distant from the north property line in the vicinity of the proposed construction on the Vachon property. It should also be noted that the

majority of the visual/sound barrier created by the existing mature forest in this area between the house and the proposed development is going to be removed, only to be replaced by young plantings that will take many years to reestablish the buffer. Therefore, has the impact of lighting and noise on the adjacent residence been evaluated to determine if there will be any significant impact to it?

- 3. How will snow removal be handled in this area so as not to impact the adjacent wetlands (salt or other ice removal chemicals) and proposed landscaping?
- 4. It is unclear on how the "island" in the middle of the proposed parking area is going to be constructed, i.e., raised island with landscaping; raised island paved with no landscaping; flush with whatever in between; etc.? Can this area be used as a rain garden to mitigate some of the runoff from the pavement?
- 5. Is there any consideration to provide some form of "tall" landscaping in the center island, considering how much impervious pavement is being proposed?
- 6. As an aid to construction, it would be helpful to include a cross-section profile from the detention basin outlet structure to just beyond the level spreader.

SHEET 5 OF 5 - DETAIL SHEET

- 1. In the "Stormwater Basin Outlet Detail," a smooth outer wall PVC pipe may be less susceptible to upheaval or degradation (breakage) by icing conditions than a corrugated type of pipe. It is recommended that this be evaluated by the designer. Furthermore, over time, ultraviolet rays in sunlight degrades unprotected plastic pipe, which causes it to lose structural integrity and stability. Considering this, concrete may be a better choice.
- 2. In the "Stone Berm" detail, what specific type of filter fabric should be used to minimize sediment transport and at the same time allow the efficient transmission of water toward the outlet structure? This should be specified in the detail. Also, what are the conditions as to when the berm should be replaced to function as designed due to sediment build up?
- 3. It is recommended that the "Silt Fence Backed with Haybales" detail title be modified to read "Super Silt Fence (Silt Fence Backed with Haybales or Wood Chip Berms)."
- 4. In the "Chain Link Fence Detail" the gauge of the fence fabric and size of the selvage should be specified and also what type of material it is manufactured from (galvanized steel, PVC coated steel, etc.). The same goes for the posts and hardware, too, and depth of bury/concrete anchorage for the posts.
- 5. In the "Stone Berm" detail, will CONNDOT crushed stone M.01.01 #3 remain stable at a 2:1 angle of repose?
- 6. In the "Slope Stabilization Detail" it is recommended that the slope be 3:1 or flatter, NOT 2:1 or steeper, as shown.
- 7. In the "Bituminous Lip Curb" detail it is recommended that the curb be formed on the binder course (locked in) for better stability/longevity, which should provide more resistance to deformation by snowplowing operations or other vehicle impacts.

Bv: Syl Pauley, Jr., P.E., NECCOG Regional Engineer

Audrey Lussier

From:Audrey LussierSent:Tuesday, July 28, 2020 3:21 PMTo:permitsct@momentumsolar.comSubject:90 Bailey Woods Road Brooklyn CT - roof and solar building permits

Hello:

I am in receipt of three permits for 90 Bailey Woods Road. No fees were enclosed with the packet: Fee schedule as follows:

Building Permit: Strip and reroof permit - fee is \$70

Building Permit for Roof Mounted Solar & Electrical (Electrical Portion is \$210, Roof Mount Solar 29 Modules is \$90 Total = \$300 for the solar and electrical combined).

Please submit two checks made payable to Town of Brooklyn --

One for \$70 (roof).

One for \$300 (electrical and solar panels).

Mail to Building Dept, Town of Brooklyn, 69 South Main St, Suite 22, Brooklyn, CT 06234.

Once payment is received the building permits will be processed.

Any questions please contact me at the number below.

Thank you

Audrey Cross-Lussier Building/Land Use Office Administrative Assistant 860-779-3411 Ext 12

Killingly Engineering Associates Civil Engineering & Surveying

P.O. Box 421 Killingiy, CT 06241 Phone: 860-779-7299 www.killingiyengineering.com

March 30, 2020

Ms. Jana Roberson, AICP Director of Community Development/Town Planner Town of Brooklyn Department of Planning Clifford B. Green Memorial Center 69 South Main Street Brooklyn, CT 06234

RE: Proposed Parking Expansion Vachon Chevrolet

Dear Ms. Roberson;

In response to NECCOG review comment on the aforementioned project, we offer the following:

Sheet 2 of 5 - Existing Conditions

1. The CGS random points referenced on the survey plan were used to establish the horizontal location of the project and have no bearing on the design. These points are not located adjacent to the site and we do not see the need or purpose of providing coordinates or creating a large-scale diagram of their locations at the expense of our client. Additionally, the Town of Brooklyn's regulations do not require such information.

Sheet 3 of 5 - Site Development Plan No. 1

- 1. A note has been added to the plan to direct the contractor to remove the anti-tracking construction entrance prior to installing the first course of pavement. This has also been noted on sheet 5 of 5 in the development schedule/sequence of operations.
- 2. The note "silt fence backed with staked haybales or wood chip berms" has been modified to read the same on all sheets.

Sheet 4 of 5 - Site Development Plan No. 2

- 1. Slopes in the detention basin have been modified so that they do not exceed 2:1 (center berm only). We have also noted that jute netting shall be installed to stabilized the basin after topsoil and seed have been applied. The center berm is designed to extend detention time in the basin and we do not anticipate erosive conditions once stabilized.
- 2. A detail for the lighting fixture with the make and model number has been added to the plans. We have also enclosed a cut sheet for the lighting as well. The chosen fixtures will be mounted no higher than 12' and are dark sky compliant. In addition, the landscaping proposed between the parking and the residences to the north will provide a very good vegetated buffer; cut sheets for the chosen plantings is included with this submission. Currently, the existing pine trees do not provide any visual buffer. As with most larger pine trees, there are minimal branches at the bottoms of the trees up to 20' or more. With regard to noise, this area will be utilized to store inventory and will not be accessed by the general public unless accompanied by a sales representative.
- 3. Snow will be stockpiled at the top of the slope adjacent to the proposed stormwater basin. Sheet 5 of the plans specify that no salt or chemical applications for snow removal shall be used.

- 4. The island in the center of the site will be depressed. We will incorporate rain garden plantings into the island to promote stormwater treatment and infiltration.
- 5. As the center island will be utilized in the capacity of a rain garden, we do feel that taller vegetation would be appropriate.
- 6. A cross section of the basin outlet has been added to the plans as requested,

Sheet 5 of 5 - Detail Sheet

- 1. The manufacturer of ADS N-12 HDPE pipe states a life expectancy of 100 years. For the upright outlet structure, the base will be embedded in concrete to anchor it in place to prevent upheaval and the depth of bury for the outlet pipe will for the most part be installed below frost level. We have utilized this design and application for dozens of projects throughout the years and we are not aware of any failures for this application. Additionally, the installation of the outlet pipe and structure in this location does not present any structural constraints (i.e. it is not an installation subject to traffic). It is our professional opinion that HDPE pipe is sufficient for this application.
- 2. For the stone berm, specifications for the filter fabric have been called out and conditions for maintenance are defined.
- 3. The silt fence detail has been modified to read "super silt fence" as requested.
- 4. The detail for the fence installation has been modified as requested. In addition, neighbors who attended the public hearing for wetlands requested an 8' fence in lieu of a 6' fence which has been accommodated.
- 5. In our experience, the 2:1 angle of repose for the DOT #3 stone is stable. Section 5-10-12 of the 2002 CT Guidelines for Soil and Erosion Control ("the 2002 Guidelines") specify slopes no steeper than 1:1 and heights no greater than 3'.
- 6. We have modified the slope stabilization detail to call for application on slopes 2:1 or flatter per 5-4-10 of the 2002 Guidelines.
- 7. Bituminous curb installation detail has been modified accordingly as requested,

We trust that the plans as modified address the March 23rd review comments. Please feel free to contact us if there are any further questions or concerns,

Sincerely e Mileck Nowmand Thibeault, Jr., P.E.

VIPER S	Job	Tuna				
	עטנ	Туре		Approvals	IIGHE (348876)987	
SPECIFICATIONS			PROD	UCT IMAGE(S)	Dato from	
 Intended Use: The Beacon Viper luminaire is available witholice of different LED Wattage confand optical distributions designed to relighting up to 400W MH or HPS. Construction: Manufactured with die cast aluminum Coated with a polyester finish that me ASTM B117 corrosion test requirements, external hardware is corrosion resista One plece optical cartridge system cord an LED engine, LED lamps, optics, and stainless steel bezel. Cartridge is held together with interne standoffs soldered to the board so the field replaced as a one piece optical cartridge system cord an LED engine, LED lamps, optics, and stainless steel bezel. Cartridge is held together with interne standoffs soldered to the board so the field replaced as a one piece optical ary polyurethane foam gasket ensures a weather-proof seal around each individent and the standoffs soldered to the board so the selected in options to obtain exter withing drivers are standard, but CD be selected in options to obtain exter withing leads for dimming controls Component-to-component wiring with luminaire may carry no more than 800 rated load and is certified by UL for u600VAC, 13A or higher. Plug disconnects are certified by UL for u600VAC, 13A or higher. Surge protection - 20kA. Optional 7-pin ANSI C136.41-2013 to photo control receptede available. C with ANSI C136.41 actomal wireless devices. Lifeshield™ Circuit - protects luminais excessive temperature range. Oper the smooth and undetectable to the driver. Shall excluse the operature range. Oper the amount and undetectable to the drivers. The device spall be able to c with other 0-10V control devices (cord sensors, external cirrumers, etc.). ChernificAtionS/LISTINGS 	ant. ant. ant. ant. casts asystem casts casts casts asystem casts casts casts casts for use asystem asystem casts for use asystem asystem casts for use </td <td>a mounting available, suded. Soat electrostatically ed, IFS finish consists of ant regimen with a and top coated with a polyester powder coat AIMA 2604 tion which includes ait spray test for nd resists cracking or STM D522 and resists to 160 inch-pounds. UL 8750 and CSA ed by the le Conservation a spec available at: ts.com/products/viperanali for more information visit:</td> <td></td> <td>A B C 75" 16.75" 11.25" mm) (425 mm) (286 mm) TING OPTIONS Side View Rectangula Side View 2-3/8" OD Sip Fit 18.75" C 23/8" Adjustable Knuckle (A) 0" tit - Sensors, photocells and wireless its should not be tilled above horizontal 22.50" Well Brecket (VIE) 24.75"</td> <td>$4 \frac{1}{8}$ $4 \frac{1}{8}$ Front $\frac{4 \frac{1}{8}}{15.0 \frac{1}{15.0}}$ $\frac{1}{15.0 \frac{1}$</td>	a mounting available, suded. Soat electrostatically ed, IFS finish consists of ant regimen with a and top coated with a polyester powder coat AIMA 2604 tion which includes ait spray test for nd resists cracking or STM D522 and resists to 160 inch-pounds. UL 8750 and CSA ed by the le Conservation a spec available at: ts.com/products/viperanali for more information visit:		A B C 75" 16.75" 11.25" mm) (425 mm) (286 mm) TING OPTIONS Side View Rectangula Side View 2-3/8" OD Sip Fit 18.75" C 23/8" Adjustable Knuckle (A) 0" tit - Sensors, photocells and wireless its should not be tilled above horizontal 22.50" Well Brecket (VIE) 24.75"	$4 \frac{1}{8}$ $4 \frac{1}{8}$ Front $\frac{4 \frac{1}{8}}{15.0 \frac{1}{15.0}}$ $\frac{1}{15.0 \frac{1}$	

8

Due to our continued efforts to improve our products, product specifications are subject to change without notice.

HUBBELL Lighting

ORDERIN	G INFORMATION	ORDERING EXAM	ple: VPS/24L-45/AM/	4w/unv/a/dbt/b	C		
VPS							
SERIES /PS Viper	LED ENGINE 24L-45 45W, LED array 36L-65 65W, LED array 48L-85 80W, LED array 60L-105 105W, LED array	2 Tyr 3 Tyr 4F (formerly 4) Tyr 4W Tyr 5QM Tyr 5R Tyr	pe 1/Front Row pe 2 pe 3 pe 4 pe 4 Wide	V31TAGE UNV 120-277V 120 120V 208 208V 240 240V 277 277V 347 347V 480 480V	BLS Bla DBT Dai DBS Dai GTT Gra LGS Lig PSS Pla WHT WH WHS WH VGT Ver GDLOR G	COLOR Ck Matte Textured ck Gloss Smooth k Bronze Gloss Smooth k Bronze Gloss Smooth phite Matte Textured ht Grey Gloss Smooth tinum Silver Smooth ite Matte Textured ite Gloss Smooth rde Green Textured PTION stom Color	OPTIONS CD Continuous Dimming F Fusing BSP Bird Spikes BC Backshield (available for FR, 2, 3, 4, 4W Optics) TB Terminal Block
	USE SIDE SHIELD ACCESS	TC Te	nnis Court	Mounting		7PR 7-Pin Recept or wireless o	IRGE OPTIONS acte only (shorting cap, photo conta ontrol provided by others) trate w(becting Con-
HSS/VP KSS/VP-: HSS/VP-:	-\$/90-FB/XXX 90° shield f -\$/90-ER/XXX 90° shield f \$/270-FB/XXX 270° shield \$/270-ER/XXX 270° shield \$/270-ER/XXX Full shield	left or right I front or back	or round po MAF Mast Arm F OD horizont K Knuckle (fo	itter (formerly SF2) fo	r 2-3/8" 5" tilt or	7PR-SC 7-Pin Recept 7PR-TL 7-Pin Recept	izcie w/Snorung Cap tacle w/Twist Lock photo control
(Refer to page VPL-AD-1	Alth notation for desired finish color) 5 for shield images) MOUNTING ACCESSON RPA3 2.4"-4.1° Round Pole RPA4 4.2"-5.3" Round Pole	Adapter for AD arm	AD3 Universal A AD4 Universal A	et rm for square pole rm for 2.4"~4.1" roun rm for 4.2" -5.3" roud rm for 5.5" -5.9" roud	nd pole		

ADDERING INFORMATION ODDERING EXAMPLE-VES/241-45/AM/AW/UNV/A/DRT/BC

VPL-AD-RPA5 5.5"-5.9" Round Pole Adapter for AD arm AD6 Universal Arm for 6.0"-6.5" round pole VPL-AD-RPA6 6.0"-6.5" Round Pole Adapter for AD arm

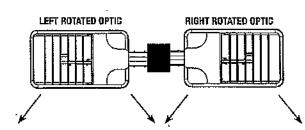
1294 PRECOMMISSIDNED SITESYNC ORDERING INFORMATION: When ordering a fixture with the StlaSync lighting control option, additional information will be required to complete the order. The SiteSync Dammissioning Form or alternate schedule information must be completed. This form includes Project location, force information, and Operating schedules. For more detailed information please wisi <u>www.hubbell-outomation.com/products/sitesyncf</u> or context Hubbell Lighting tech support at 854-878-1000.

SiteSync fixtures with Molion control (SWPM) require the mounting freight of the fixture for selection of the leas,

Examples: VFS/24L-55/4KJ /3/UNV/A/DBT/5WP/ VPS/24L-55/4K7/3/UNV/A/DBT/SWPM-40F/

¹ Only available with 1A, 2, 3, 4, 4W and 5R distributions

SiteSync only SileSyne with Motion Control





Beacon Products = 2041 58th Avenue Circle East Bradenton, FL 34203 = Phone: 864,678,1000 HUBBELL Due to our continued efforts to improve our products, product specifications are subject to change without notice. © 2028 BEACON PRODUCTS, All Rights Reserved • For more information visit our website: www.beaconproducts.com • Priced in USA MARCH 27, 2020 10:25 AM

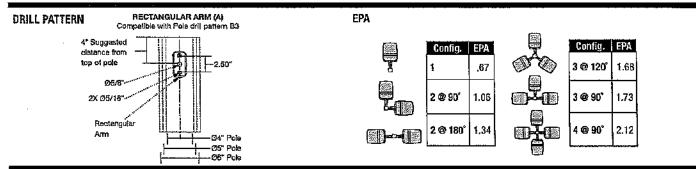


PERFORMAN	ce data	AMB					
			amber 590nm (std.)				
	SYSTEM	DISTRIBUTION					
# LED'S	WATTS	TYPE	LUMENS	LPW	B	U	Ģ
1.		FR	1238	28	0	0	0
		2	1194	27	0	0	0
		3	1171	26	0	0	1
		4	1152	26	0	0	Ū,
24	45W	4W	1127	25	Q	0	1
		5QM	1173	26	1	0	0
		5R	1181	26	1	0	
		5W	1260	28	1	0	Ō
		TC	1204	27	0	0	0
		FB	1857	29	0	0	0
		2	1791	28	0	0	0
		3	1757	27	0	0	1
36	65W	4	1728	27	0	0	1
30	0.00	4W	1690	26	0	0	1
		50M	1759	27	1	D	0
		5R	1771	27	1	0	1
		5W	1726	27	1	0	0
		FR	2476	29	0	0	0
		2	2389	28	1	0	1
		3	2343	28	0	0	1
		4	2304	27	Q	0	1
48	85W	4W	2254	27	0	0	Π
		5QM	2346	28	1	0	0
		5R	2362	28	11	0	1
[5W	2301	27	2	0	1
		TC	2408	28	0	0	0
		FR	3095	29	1	0	0
		2	2986	28	1	0	1
	[3	2927	27	1	0	2
		4	2880	27	Ð	0	1
60	105W	4W	2817	26	0	0	1
		5QM	2933	27	1	0	0
		5R	2953	28	2	D	2
		5W	2879	27	2	0	1
		TC	3011	28	0	0	1

			AMB				
			ami	ber 590nn	n (std	.)	
	SYSTEM	DISTRIBUTION					
# LED'S	WATTS	TYPE	LUMENS	LPW	.В.	U	Ģ
		FR-BC	1064	24	0	0	0
		2-BC	880	20	0	0	0
24	45W	3-BC	802	18	0	0	0
24	4317	4-BC	887	20	0	0	0
		4W-BC	2014	45	0	0	1
		TC-BÇ	930	21	0	0	0
		FR-BC	1596	25	0	0	Q
36		2-BC	1320	20	0	0	0
	65W	3-BC	1202	18	0	0	0
		4-BC	1330	20	0	0	: 0
		4W-BC	2014	31	0	0	1
		TC-BC	1396	21	0	0	0
		FR-BC	2128	25	0	0	0
	1	2-BC	1761	21	0	0	0
		3-BC	1603	19	0	0	1
46	85W	4-BC	1774	21	0	Ð	1
		4W-8C	1450	17	0	0	0
		TC-BC	1861	22	0	0	0
		5R	2362	28	1	0	1
		FR-BC	2661	25	0	0	0
•		2-BC	2201	21	0	0	0
	1	3-80	2004	19	0	0	1
60	105W	4-BC	2217	21	0	0	1
		4W-BC	1813	17	0	0	1
		TC-BC	2326	22	0	0	0
		5R	2953	28	2	0	12



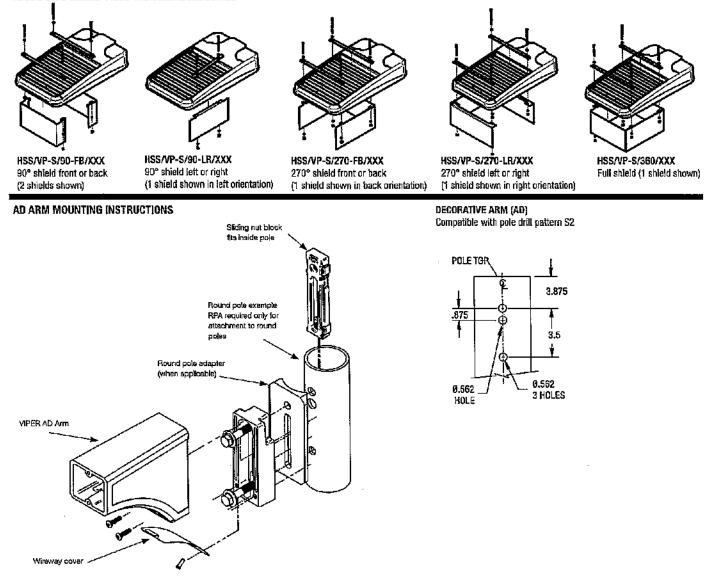
HUBBELL Lighting (HUBBELL)



TENON TOP POLE BRACKET ACCESSORIES (Order Separately) (2.3/8" OD tenon)

Catalog Number	Description
SETAVP-XX	Square tenon adapter (4 at 90°) for A - Rectangular Arm mounting option only
RETAVP-XX	Round tenon adapter (4 at 90°) for A - Rectangular Arm mounting option only
TETAVP-XX	Hexagonal tenon adapter (4 at 90°) for A - Rectangular Arm mounting option only
SETA2XX	Square tenon adapter (4 at 90°) for AD - Universal Arm mounting option only
RETA2XX	Round tenon adapter (4 at 90°) for AD3 - Universal Arm mounting option only
TETA2XX	Hexagonal tenon adapter (3 at 120°) for AD - Universal Arm mounting option only

HOUSE SIDE SHIELD FIELD INSTALL ACCESSORIES





Beacon Products • 2041 58th Avenue Circle East Bradenton, FL 34203 • Phone: 864,678,1000 Due to our continued efforts to improve our products, product specifications are subject to change without notice. © 2020 BEACON PRODUCTS, All Pliphis Reserved • For more information visit our website: www.heaconproducts.com • Printed in USA MARCH 27, 2020 10:26 AM

TOWN OF BROOKLYN Inland Wetlands Commission Regular Meeting Minutes Virtual Webex Meeting June 9, 2020

Call to Order: The meeting was called to order at 6:01 p.m.

Members Present: Jeffrey Arends, Demian Sorrentino, Richard Oliverson, George Sipila, James Paquin.

Absent: None.

Staff Present: Margaret Washburn, Wetlands Agent, Rick Ives, First Selectman, Audrey Cross-Lussier, Recording Secretary.

Also Present: Norm Thibeault, David Held, Paul Archer, Jeffrey Weaver, Don Dubois.

Roll Call: All members present stated their name for the record.

Seating of Alternates: None.

Public Commentary: None.

Additions to the Agenda:

A motion was made by Jim Paquin to add the following applications under new business as # 4, 5, 6:

#4 060920A Paul R. Lehto, Allen Hill Road, Map 32, Lot 148, RA Zone; Excavation of sand and gravel.

#5 060920B VBL Properties, LLC, Beecher Road, Map 22, Lot 38, RA Zone; 5-Lot Subdivision.

#6 060920C A. Kausch & Sons, Tripp Hollow Road, Map 15, Lot 4, RA Zone; 2-Lot Subdivision; single family homes, driveways, septic, well and minor gradings.

Demian Sorrentino seconded this motion. Discussion held.

Mr. Sorrentino asked if applications # 5 and 6 had delineation reports and stamped/signed by professional engineer Robert Deluca? Paul Archer commented that the plans were e-mailed to Ms. Washburn this afternoon. Ms. Washburn commented that part of application 060920B for VBL Properties, Beecher had been left blank, specifically the section with regards to cubic yards of filling of wetlands. Mr. Archer commented he will complete the application and stated there is no work being done in the wetlands.

No further discussion. All in favor. The motion passes.

Approval of Minutes:

1. Regular Meeting Minutes March 10, 2020. Minutes were approved as written with no changes made.

Continued Public Hearings:

1. 021120B Vachon Brooklyn, LLC, 512 Providence Road, Map 41, Lot 13A/14, PC Zone; Construction of (2) 16 ft. wide access driveways to access proposed new vehicle storage lots. Drive to the larger of the two proposed parking areas will be in an area historically used for an agricultural crossing. (public hearing suspended due to COVID 19)

Norm Thibeault, Killingly Engineering represents the application. Mr. Thibeault stated that the plans were reviewed by NECCOG Engineer Syl Pauley. Mr. Thibeault addressed Mr. Pauley's comments on each page, adjusted the plans and provided written responses to written comments.

Chairman Arends asked Mr. Thibeault how many parking spots are proposed. Mr. Thibeault commented 140 to 150 in the back. The spaces will not be lined. Chairman Arends asked if there will be an area designated for snow. Mr. Thibeault stated snow removal will be pushed to the edge of the parking lot towards the stormwater basin. The basin is capable of handling a 100-year storm. Mr. Thibeault discussed the wetland basin and vegetative mitigation.

Chairman Arends asked if there was any public commentary to be heard. With no further discussion or public commentary Chairman Arends asked for a motion to close the public hearing.

A motion was made by Jim Paquin to close the public hearing. Demian Sorrentino seconded the motion. Discussion held. All in favor. The motion passed unanimously.

Old Business:

1. 021120B Vachon Brooklyn, LLC, 512 Providence Road, Map 41, Lot 13A/14, PC Zone; Construction of (2) 16 ft. wide access driveways to access proposed new vehicle storage lots. Drive to the larger of the two proposed parking areas will be in an area historically used for an agricultural crossing. (public hearing suspended due to COVID 19). Chairman Arends stated he would like to see as a condition that only new vehicles are stored in the back lot.

A motion was made by Demian Sorrentino to approve application 021120B Vachon Brooklyn, LLC, 512 Providence Road, with proposed special conditions (in addition to standard conditions):

1. The detention basins shall be constructed, stabilized, and seeded before the parking lots constructed.

2. Install the sediment/erosion controls as shown on the approved plans and call the Wetlands Enforcement Officer at (860) 779-3411, extension 31, for an inspection prior to starting any earth disturbance activities. Written approval of the sediment/erosion controls must be given by the Wetlands Enforcement Officer prior to starting any earth disturbance activities.

3. Only new vehicles, no used or trade in, or employee parking allowed in the parking lots.

4. Invasive species to the greatest extent possible the contractor will eradicate invasive species during construction.

Jim Paquin seconds the motion. Discussion held. All in favor. The motion passes unanimously.

2. 121019A Hearing for violation at 260 Woodward Road, Owner Richard and Sandra Duval. Cease and Desist order on 12/2/19 for site work consisting of excavating material from the channel of Sandy Brook, excavating material from an existing ford in Sandy Brook, and depositing excavated material on the bank of Sandy Brook, in the upland review area and/or wetlands.

Ms. Washburn and Richard Oliverson visited the site on 4/23/20, photographs were taken. Members review the photographs. Discussion held. Mr. Paquin asked if the picture were from February or now. Ms. Washburn stated the latest pictures are from 4/23/20. The older pictures are from 11/25/19. Chairman Arends asked Mr. Oliverson if he was satisfied with what he saw at the site. Mr. Oliverson commented a retaining wall appeared with logs present and stabilized area behind it. The stones are back in, there is now a ford back in the water.

A motion was made by Jim Paquin to close 121019A hearing of violation at 260 Woodward Road, Richard and Sandra Duval and lift the cease and desist order. Richard Oliverson seconds this motion. No discussion held. 4 in favor, 1 abstention. Motion passes.

3. 031020A Darko Krsulic/Owner, Evan Sigfridson/Applicant 293 Hartford Rd, Map 16, Lot 39, RA Zone; Demolish remainder of collapsed coop, dig, and pour frost walls for proposed 24 x 32 ft accessory building.

Ms. Washburn spoke with Mrs. Krsulic who stated they are suspending the project due to COVID-19 concerns. There have been no delineation or revised site plan prepared to date.

A motion was made by Demian Sorrentino to deny without prejudice and allow the applicant to reapply when they have appropriate material to submitted with the application. Richard Oliverson seconds the motion. Discussion held. Jim Paquin asked if they will apply the old application fee collected or require a new fee paid? Commission members agree to apply the old application fee collected.

Demian Sorrentino amends his motion to include waiver of fee when application is made.

George Sipila seconds. No further discussion. All in favor. The motion passes unanimously.

4. 031020B Jeffrey Weaver, Day Street, Map 43, Lot 6, RA/R30 Zone, 6 lot subdivision, work in upland review area, septic system, driveway, residential house, well, grading.

Paul Archer, Archer Surveying represents the applicant Jeffrey Weaver. This is a proposed 6-Lot subdivision west of the 10-Lot subdivision on Day Street. Four of the lots are totally outside upland review area. Two lots #1 and #6 will have work within the 125-ft upland review area. There is Department of Health approval. Plans have been submitted to Ms. Washburn. Four of the lots are in RA/30 zone, two lots RA Zone. All septic systems are engineered, everything pitches away from road. There are four frontage lots, two rear lots.

Mr. Sorrentino has reviewed the plans and wetlands reports and feels there is no issue with the application, there is essentially little change effecting inland wetlands and watercourses when lots developed. Jim Paquin agrees. Mr. Paquin reviewed Syl Pauley's notes 1-6, (page 63 of 120 of agenda items) and does not feel the need to address these at all.

Chairman Arends asked Ms. Washburn if she has looked at the site. Ms. Washburn has no issues.

A motion was made by Jim Paquin approve 031020B Jeffrey Weaver, Day Street, Map 43, Lot 6, RA/R30 Zone, 6-Lot subdivision, work in upland review area, septic system, driveway, residential house, well, grading with standard IWWC conditions. Demian Sorrentino seconds this motion. No further discussion. All in favor. The motion passes unanimously.

New Business:

1. Ernest Robillard, 509 Hartford Road, Agricultural exemption for two new barns near ponds.

Ms. Washburn commented that Mr. Robillard would like to speak to the Commission directly after the virtual meetings have ended and in person meetings begin. Discussion tabled to the next regularly held in person meeting.

2. 051220A Patrick Riley, 211 Windham Road, Map 8, Lot 6-3, RA Zone; Construction of single-family dwelling, driveway, well, septic system, grading, tree clearing within 85 feet of a wetland.

Ms. Washburn issued agent approval on this application. Ms. Washburn would like the Commission to be notified. Mr. Riley advertised the legal notice in the Turnpike Buyer.

3. DR20-002 Grant Hill Road, Map 4, Lot 4 Timber Harvest, Michael Sokolowsky/Owner, Donald Dubois/Forester.

Don Dubois of Dubois Forestry and Land Management represents the applicant Michael Sokolowsky. Property located on the Brooklyn/Hampton Town Line Map 4 Lot 4. Part of a forest management plan approved by USDA, NRC. Purpose of the harvest is to remove dead and dying oak trees due to gypsy moth. The aerial maps describe a salvage thinning. There was a previous operation many years ago and there was a log loading area off of Grant Hill Road. That portion of Grant Hill Road where there is frontage is discontinued. The Town maintains a cul-desac 200-ft on the left is an access point. They would harden this proposed area with rip-wrap to make a tracking pad. There is an intermittent and perennial brook indicated on the maps. Two crossings are indicated on the map. Temporary corduroy bridge maps during operation are proposed. Upon completion bridges will be removed. Mr. Dubois reviewed the skid trails, Stand 1 and Stand 2 of the timber harvest operation. There will be 120 board feet and 250 cords. They will start after 9/1/20 finish by 4/15/21.

Chairman Arends asked if the Commission have any questions. No questions asked.

A motion was made by Demian Sorrentino to approve a declaratory ruling using agriculture as of right for application DR20-002 Grant Hill Road, Map 4, Lot 4, Timber Harvest, Michael Sokolowsky/Owner, Donald Dubois/Forester. Jim Paquin seconds the motion. No discussion held. All in favor. The motion passes unanimously.

4. 060920A Paul R. Lehto, Allen Hill Road, Map 32, Lot 148, RA Zone; Excavation of sand and gravel.

David Held, Provost of Rovero represents the applicant Paul R. Lehto. This is an expansion of a project the Commission approved in October of 2018. The project never went forward. This application includes an expansion to the south and to the east towards the Dick Regis property and Town of Brooklyn property bordering the Quinebaug River. The delineation done on the original plans were done by Joe Theroux in 2016. There is 175-ft. regulated area from edge of actual wetlands. There is a watercourse runs through wetlands system. They are proposing substantial topsoil area, a proposed paved and curved equipment area. Phase 1 southerly area is 4.1 acres with 63,000 cubic yards removed of sand and gravel. On the northerly edge a permanent sedimentation basin is proposed throughout the project. Proposed cuts in Phase 1 area maximum depth 25-feet with Phase 1 hillside re-sloping and regrading. The entire work area drains inward with little concern for E and S down gradient. Phase 2 area is essentially the same limits of what was previously approved. There will be clearing 8 to 10 ft. from the delineated wetlands edge. The excavation will maintain a 50-ft. buffer which will remain undisturbed. No additional disturbance is proposed. Phase 2 area 2.6 acres with 27,000 cubic yards removed. There is a valid wetlands permit for the Phase 1 area, looking to add in the expansion. All excavated material transported off site is on existing haul road referred to Riverwalk Drive. This is a private drive which serves condominiums and apartments. This intersects with Allen Hill

Road. There will be no processing of material on site, no sales to the public to material on site. The intention is to sell material in place, and this would be relatively short-term project to who he is selling to and get restoration accomplished.

Zoning regulation changes since 2018 - a few upgrades to the plans were made to keep up with the new zoning regulations. A fueling pad was added outside the regulated area. Hay bales added for restoration, and quantities and qualities of topsoil used, and vegetative seeding requirements were added. As the respective phases are completed the restoration would be undertaken under the first available growing season following excavation. Due to the limited size of the project both phases 1 and 2 will be bonded.

The applicant is looking for an acceptance of the application and a determination of no significant activity.

Mr. Sorrentino asked about woody debris on the slope. Mr. Held stated there is additional clearing there on the easterly edge, within existing clearing limits. Mr. Held addresses Mr. Sorrentino's concerns.

Ms. Washburn commented there were earlier complaints received from Riverwalk Drive residents with regards to 3 to 4 truckloads coming out. Mr. Lehto removed some big boulders for restoration. Ms. Washburn commented there has been no complaints for several months now. Mr. Lehto had never paid the PZC required bond for the last approved project. Discussion held.

A motion was made by Demian Sorrentino to receive application 060920A Paul R. Lehto, Allen Hill Road, Map 32, Lot 148, RA Zone, Excavation of sand and gravel and continue to next regularly scheduled monthly meeting for consideration. Jim Paquin seconds the motion. No discussion held. All in favor. The motion passes unanimously.

The commission determines there is no significant activity, no public hearing will be required.

5. 060920B VBL Properties, LLC Beecher Road, Map 22, Lot 38, RA Zone; 5-Lot Subdivision.

Paul Archer, Archer Surveying represents the applicant. This is a 14-acre parcel on the south side of Beecher Road. A free split has been done and a house was built on the westerly end of the road. The main concern is Blackwell's Brook runs through the easterly side of the project. There is one proposed lot on the westerly side in the upland review area. Two proposed lots in the middle are not in upland review, one lot on the west side of Blackwell's Brook has little in upland review area and one lot on Rukstela Road which goes to the landfill is where work will be done in the upland review area. Two of the five lots have non-engineered septic systems. They are awaiting NDDH approval. The applicant is in the beginning phase, looking for acceptance to return next month to discuss.

A motion was made by Jim Paquin to receive application 060920B VBL Properties, LLC Beecher Road, Map 22, Lot 38, RA Zone; 5-Lot Subdivision. Demian Sorrentino seconds this motion. Discussion held.

Demian Sorrentino commented there are no wetland flag numbers on the conceptual development plan, a report submitted from soil scientist is required. Mr. Archer agrees. Mr. Sorrentino also asked to be sure the Commission has plans stamped by the PE.

All in favor. The motion passes unanimously.

6. 060920C A. Kausch & Sons, Tripp Hollow Road, Map 15, Lot 4, RA Zone; 2-Lot Subdivision; single family homes, driveways, septic, well and minor grading.

Paul Archer, Archer Surveying represents the applicant. This is a 4-acre lot on the westerly side of Tripp Hollow Road. The wetland flags are on the development plan. This is a simple 2-lot subdivision with no work in the wetlands but in the upland review area.

Mr. Sorrentino questions the proximity of the septic systems to the wetlands. Soil scientist to submit a report. Mr. Sorrentino asked if there is standing water in wetland? Mr. Archer stated no, there is not.

Mr. Archer commented the septic systems are 40 plus feet away from wetlands. Mr. Sorrentino feels they are very close. Mr. Archer commented the wetland flags are new, Joe Theroux did them back in February 2020. The wetlands were delineated after the test pits were done.

Jim Paquin commented that the primary system for Lot 4A-1 be moved closer to the driveway turn around, move the house closer to the right. Mr. Archer commented the setbacks are tight. Discussion held.

Mr. Archer will get permission from the property owner for Commission members to visit the site.

A motion was made by Demian Sorrentino to receive application 060920C A. Kausch & Sons, Tripp Hollow Road, Map 15, Lot 4, RA Zone; 2-Lot Subdivision; single family homes, driveways, septic, well and minor grading and table discussion to next regularly scheduled meeting. Richard Oliverson seconds the motion. Discussion held. All in favor. The motion passes unanimously.

Communications:

1. Budget Update: Commission Members Reviewed the Budget.

Mr. Sorrentino asked if any advertising has been done to recruit new Commission Members. Ms. Washburn commented that Ms. Diamen has had health issues and not pursuing joining the Commission at the present time. No advertising has not been done at this time.

2. Wetlands Agent Monthly Report:

Complaint Map 18, Lot 28 Woodward Road, William and Kathy Perron. Excavating gravel, mine and cutting hard wood close to Blackwell's Brook. There is no approved residence located at this site, there are several sheds/out-buildings.

Ms. Washburn is going to send out a wetlands violation letter and request him to attend July meeting. She will send out the letter 10 days prior to the meeting as required by law.

Mr. Paquin asked if she has tried to contact him. Ms. Washburn has had a hard time locating the Perron's whereabouts.

Mr. Paquin suggests duct taping the violation to his machine on site, send letter by certified mail to last known address.

Ms. Washburn will send out order to stop work now and send out violation letter 10 days prior to next month's meeting.

Public Commentary: None

Adjourn: A motion was made by Jim Paquin to adjourn the meeting at 7:21 p.m. Richard Oliverson seconds this motion. No discussion held. All in favor. The motion passes unanimously.

```
Audrey Cross-Lussier, Recording Secretary
```

Killingly Engineering Associates

P.O. Box 421 Killingly, CT 06241 Phone: 860-779-7299 www.killinglengineering.com

July 28, 2020

Proposed Parking Expansion

Vachon Brooklyn, LLC Vachon Chevrolet Providence Road (Route 6) Brooklyn, CT

	JUL	EO	V	E	n
D	JUL	28	202	0	U
By_					

APPLICATION PACKAGE CONTENTS – Special Permit Modification

1. Application fee:

\$300.00 (Site Plan Review Fee) \$100.00 (Special Permit Fee) <u>\$ 60.00 (State Fee)</u> \$460.00 Total Fee

- 2. 4- full sized sets of plans- Dated: 1/07/2020 Revised to: 3/31/2020
- 3. Special Permit Application
- 4. Site Plan Review Application
- 5. List of adjacent land owners including across the street
- 6. Environmental Impact Statement (Soil Scientist Wetlands Impact Report)
- 7. IWWC Approval minutes

Note: Drainage Calculations & Report previously submitted and approved by Town Engineer

					То	wn of B	rooklyn				
General Ledger - On Demand Report			Fi	scal Year:	2019-2020	From Date:7/	(1/2019 To Date	e:6/30/2020			
Account M	ask: 10	050000002021	3	-		Account Ty	pe: All		🗹 Print Detai	il 🔲 Include PreEnc	umbrance
			🗌 Pr	int accounts with zero ba	lance 🗌	Include Ina	ctive Accounts	Filter Encumbra	ance Detail by Date F	Range Bud	get Balance
Account Nu	mber / D	escription				Budget	Range To Date	YTD	Balance	Encumbrance % Rer	naining Bud
1005.00.00	00.20213	3				\$0.00	(\$20,980.00)	(\$63,787.47)	\$63,787.47	\$0.00	\$63,787.47
Developme	ent Appli	ications									0.00%
Transaction D	etail										
Date	Entry	Check Number	Deposit No.	Invoice Number	PO Number	Voucher	Group ID	Memo	Vendor	Amour	nt Journal
09/12/2019	343	0	1056		0		Develo	pment Applications		-\$1,370.0	0 DP
09/30/2019	436	0	1094		0		Develo	pment Applications		-\$760.0	0 DP
10/28/2019	563	0	1158		0		Develo	pment Applications		-\$11,220.0	0 DP
11/13/2019	638	0	1200		0		Develo	pment Applications		-\$260.0	0 DP
12/12/2019	766	60132	0	12/04/2019	20200907	1061	Reimb	urse overpayment for pla	an David Cameron Ford	\$250.0	0 AP
01/24/2020	993	0	1370		0		Develo	pment Applications		-\$760.0	0 DP
05/22/2020	1555	0	1640		0		Develo	pment Applications		-\$460.0	0 DP
06/05/2020	1634	0	1688		0			pment Applications		-\$2,220.0	0 DP
06/12/2020	1651	0	1696		0			pment Applications		-\$1,070.0	0 DP
06/29/2020	1734	0	1735		0			pment Applications		-\$3,110.0	
	-				-				Detail Total:	-\$20,980.00	
	FUND:	1005				\$0.00	(\$20,980.00)	(\$63,787.47)	\$63,787.47	\$0.00	\$63,787.47

1

		Town of B	rooklyn					
General Ledger - On Dema	Fiscal Year:	2019-2020	From Date:7/1/2	2019 To Dat	te:6/30/2020			
Account Mask: 100500000020213		Account Ty	ype: All	Print Detail Include PreEncumbrar				
	Print accounts with zero balance	Include Ina	ctive Accounts	Filter Encumbranc	e Detail by Date	Range	Budget Balance	
Account Number / Description		Budget	Range To Date	YTD	Balance	Encumbrance	% Remaining Bud	
	Grand Total:	\$0.00	(\$20,980.00)	(\$63,787.47)	\$63,787.47	\$0.00	\$63,787.47	

End of Report

2